

# Central Geelong Framework Plan

A plan for the heart of Djilang

February 2023

**VICTORIA**  
State  
Government

Department  
of Transport  
and Planning

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## Acknowledgement of Country

The Victorian Government proudly acknowledges Victoria's Aboriginal communities and their rich culture, and pays its respects to their Elders past and present. We also recognise the intrinsic connection of Traditional Owners to Country and acknowledge their contribution in the management of the land, water and resources.

We acknowledge the Wadawurrung People as Geelong's First People and as the Traditional Owners and custodians of the land and water on which we rely. We recognise and value the ongoing contribution of the Wadawurrung community to Victorian life and how this enriches us. We embrace the spirit of reconciliation, self determination and ensuring an equal voice.

Image of Uncle Byron Powel conducting a smoking ceremony  
Source: *City of Greater Geelong*

# Executive summary

## Key elements

- 30 year plan to support growth and development.
- Activity Centre Zone expanded to approximately 200 hectares.
- Unique growth and development opportunities within 9 distinct precincts.
- Planning controls to provide 1.67 million square metres of new floorspace.
- Space for 60,000 jobs and 16,000 residents by 2050.

## The vision

*“By 2050, Central Geelong is home to 16,000 people and 60,000 jobs. It is the heart of Djilang (Geelong), Victoria’s second largest city, with strengths in health, services, education, and events. People are connected to its waterfront, high amenity lifestyle and clever and creative culture.”*



## The purpose

This Framework Plan is a long-term strategic land use plan for the development of Central Geelong to 2050. It will help guide investment and development by the Victorian Government, the City of Greater Geelong and the private sector, creating certainty for the community, landowners, developers and businesses.

This Framework Plan will support Central Geelong's transition into a modern, connected, liveable, prosperous, inclusive, healthy and environmentally sustainable place that is home to 16,000 residents and hosts for 60,000 jobs across nine interconnected precincts.

## The opportunity

Located on the Corio Bay foreshore about 75 kilometres from Melbourne, Central Geelong forms the urban centre of Geelong, Victoria's second-largest city outside Melbourne. Geelong provides centralised civic, health, education, transport and retail services to local and regional residents, and it is the gateway to south-west Victoria.

Geelong is a convergence site for regional and interstate rail and coach networks, state roads, local public transport and port logistics, with Central Geelong at its heart. It provides the most jobs of any place in the region, and is home to major civic and private enterprises and headquarters.

Identified as a UNESCO City of Design, Central Geelong has a long and proud history of innovation and design, which can be seen in its many artistic and cultural events and institutions today.

As a destination and gateway for domestic and international visitors, Central Geelong and its waterfront provide unique experiences and attractions which attract many people.

Central Geelong is projected to grow substantially in to the future. Strategically managing this growth is essential to ensure that high quality design outcomes are achieved which support a vibrant city life where one can move about easily and provides a place for people to live, work and play.

# The objectives

## A thriving economy and job creation

- Establish Central Geelong as a national, state, and regional centre accommodating 60,000 jobs.
- Maintain and enhance Central Geelong's role as a vibrant hub for retail, hospitality arts and culture.
- Create diverse housing choice to accommodate 16,000 residents.
- Provide adequate social and affordable housing to meet the needs of a growing community.
- Strengthen Central Geelong's role a clever, creative city and a destination for visitors and major events.



## A connection to place

- Ensure built form outcomes reflect the preferred character and amenity of each precinct.
- Built form is of an appropriate scale when viewed from both its local surrounds and from a distance.
- Master plan and coordinate the development of large development sites.
- Celebrate, protect, and appropriately interpret Aboriginal cultural values.
- Retain, preserve, and protect the heritage-built form fabric and character of Central Geelong.



## Getting around with ease

- Make Central Geelong a more walkable and safer city.
- Provide safe, direct, and connected cycling routes.
- Refine and undertake phased uplifts to the public transport network in Central Geelong.
- Develop the Geelong Station precinct into a world class transport hub.
- Strategically manage the provision of car parking.



## A vibrant city life

- Ensure buildings provide a high quality and interactive interface to the street.
- Protect existing, and provide new high quality, accessible open space.
- Provide high quality streetscape environments.
- Provide the social infrastructure and utilities to meet growing needs.
- Encourage sustainable development that responds to climate change.





## The outcomes

Geelong will continue to be Victoria's premier regional city. At its heart, Central Geelong will continue to grow and develop into a contemporary regional city.

Central Geelong has significant opportunity to capitalise on the growth and development opportunity being present. Nine precincts have been identified which will grow and evolve around an anchoring use reflected in its name will continue to maintain Central Geelong as a major regional centre for goods and services.

This Framework Plan and its corresponding planning controls have been designed to provide 1.67 million square metres of new floorspace to accommodate future residential and commercial growth. The built form controls will support this growth while managing the form and scale of buildings ensuring that high quality design outcomes drive the evolution of Central Geelong's character overtime while protecting its existing assets and respecting its past.

This Framework Plan provides clear direction to deliver high-amenity public streets and open spaces to achieve a safe, high-quality experience of the city centre, leveraging its proximity to the natural assets of Corio Bay and its magnificent portfolio of heritage buildings and places. Achieving less congestion via a clearer road hierachy and less dependancy on cars to access Central Geelong will be key to achieving this.

## Implementation

A suite of planning controls have been prepared to realise the objectives and strategies in this Framework Plan which have been implemented into the Greater Geelong Planning Scheme via Amendment C431gge.

Key elements of the planning control package include:

- provision for flexibility in land use to support active streets and a strong retail core
- encouragement for dwelling diversity and type across Central Geelong
- variable building height controls, ranging from 16m to 60m with strong tower management requirements
- protections to ensure natural light to public open space and key streets and laneways across Central Geelong
- inclusion of a number protections to respect and protect building heritage
- street wall heights and upper storey setback requirements to ensure development is presented at a human scale
- plot ratios for strategic development sites to ensure each site maximises its development potential.

This Framework Plan and the suite of planning controls supporting it will be reviewed perdioically to ensure sustainable growth and development continue to occur within Central Geelong over the next 30 years.



Figure 1. Central Geelong Strategic Framework







# Setting the Scene

**This section provides an overview of Central Geelong as it is today. It explains how the Framework Plan was prepared and the opportunities and challenges facing Central Geelong.**

# Introducing Central Geelong

**Djilang (Geelong) is located on the traditional lands of the Wadawurrung People, who have cared for Country for tens of thousands of years.**

**Today, Geelong is Victoria's second largest city, and it is steadily growing. Over the past 185 years, Central Geelong has grown to be the business, cultural and event heart of the Geelong region.**

**With its stunning, north-facing waterfront, beautiful open spaces and great transport access, Central Geelong is home to a diverse range of enterprises, including retail, hospitality, professional services, accommodation, attractions, education, arts and culture enterprises.**







## About Central Geelong

Covering an area of some 200 hectares, Central Geelong is prominently located on the southern shores of Corio Bay, about 75 kilometres from Melbourne.

Central Geelong forms the urban centre of Geelong, which is Victoria's second-largest city. The city provides civic, health, education, transport and retail services to local and regional communities. It is also a gateway to the Great Ocean Road and the Bellarine Peninsula regions of Victoria.

Central Geelong has several transport modes and routes that converge, making it an important regional transport hub. These include regional and interstate rail and coach networks, local and sub-regional bus routes and major freight and vehicle transport routes.

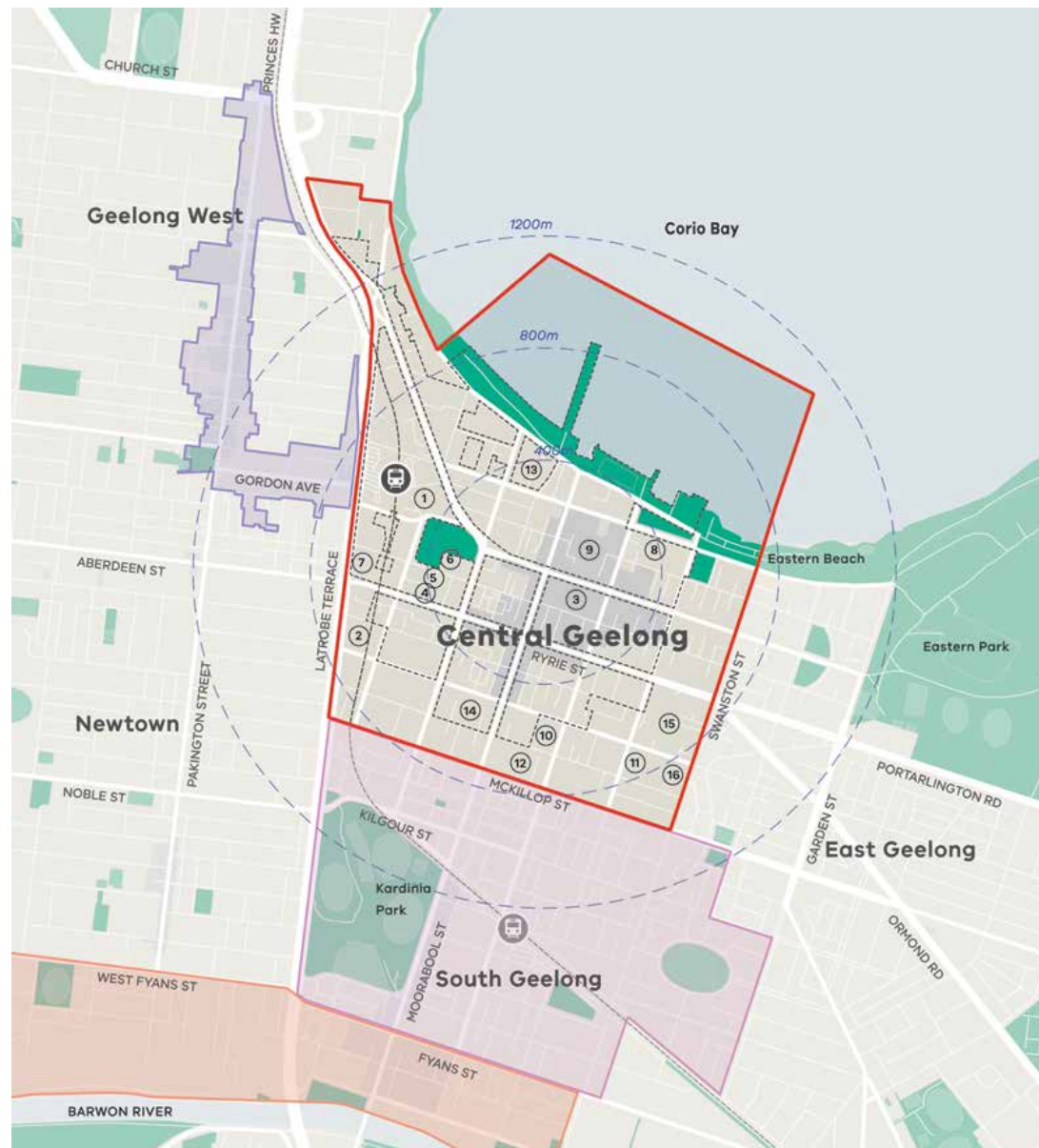
The area is an important employment hub for the region, with major public and private enterprises choosing Central Geelong as a base of operation, in particular, the health, medical and education sectors.

Central Geelong is also a destination for domestic and international visitors, its waterfront is a major attraction, offering a range of experiences.

Recently identified as a UNESCO City of Design, the city has a long history of innovation and design, which is evidenced by its many artistic and cultural events and institutions.

Central Geelong is projected to grow substantially into the future. Strategically managing this growth is vital for its future success.

Figure 2. Key sites and context



## Legend

- Existing Activity Centre Zone
- Existing open space
- Existing retail areas
- Train Station
- Central Geelong (Study Area)
- South Geelong UDF Area
- West Fyans Structure Plan Area
- Pakington Street (Geelong West) and Gordon Avenue UDF Area

## Places

- ① Police & Magistrates
- ② Matthew Flinders SC
- ③ Market Square
- ④ Barwon Health
- ⑤ Geelong Performing Arts Centres
- ⑥ Library & Art Gallery
- ⑦ The Gordon
- ⑧ Council offices
- ⑨ Westfield Geelong
- ⑩ St Marys Church
- ⑪ Newland Army Barracks
- ⑫ St Mary's Primary School
- ⑬ Deakin University
- ⑭ St John of God Hospital
- ⑮ University Hospital
- ⑯ Old Geelong Goal

## Central Geelong, a place for a city

In March 1837, the Governor of the Colony of New South Wales, Sir Richard Bourke, visited what is now known as Central Geelong. Impressed by the locality, he remarked that the 'beautiful flat behind Corroia [Corio Bay] would answer well for a township if fresh water could be obtained'.

In 1838, Robert Hoddle instructed Henry Smythe to survey the proposed township at Geelong and present a plan to the Governor.

Comprising two distinct areas based on a rectangular grid layout, the southernmost area was subdivided into eight sections and was to be the main residential area beside the fresh water source of the Barwon River. The commercial section was to front Corio Bay and comprise three sections. A main road — Bellerine Street — would link 'North' and 'South' Geelong.



Figure 3. Geelong town plan, circa 1838

Source: VPRS 8168/P5, item Sydney G15: Geelong, Public Record Office Victoria.



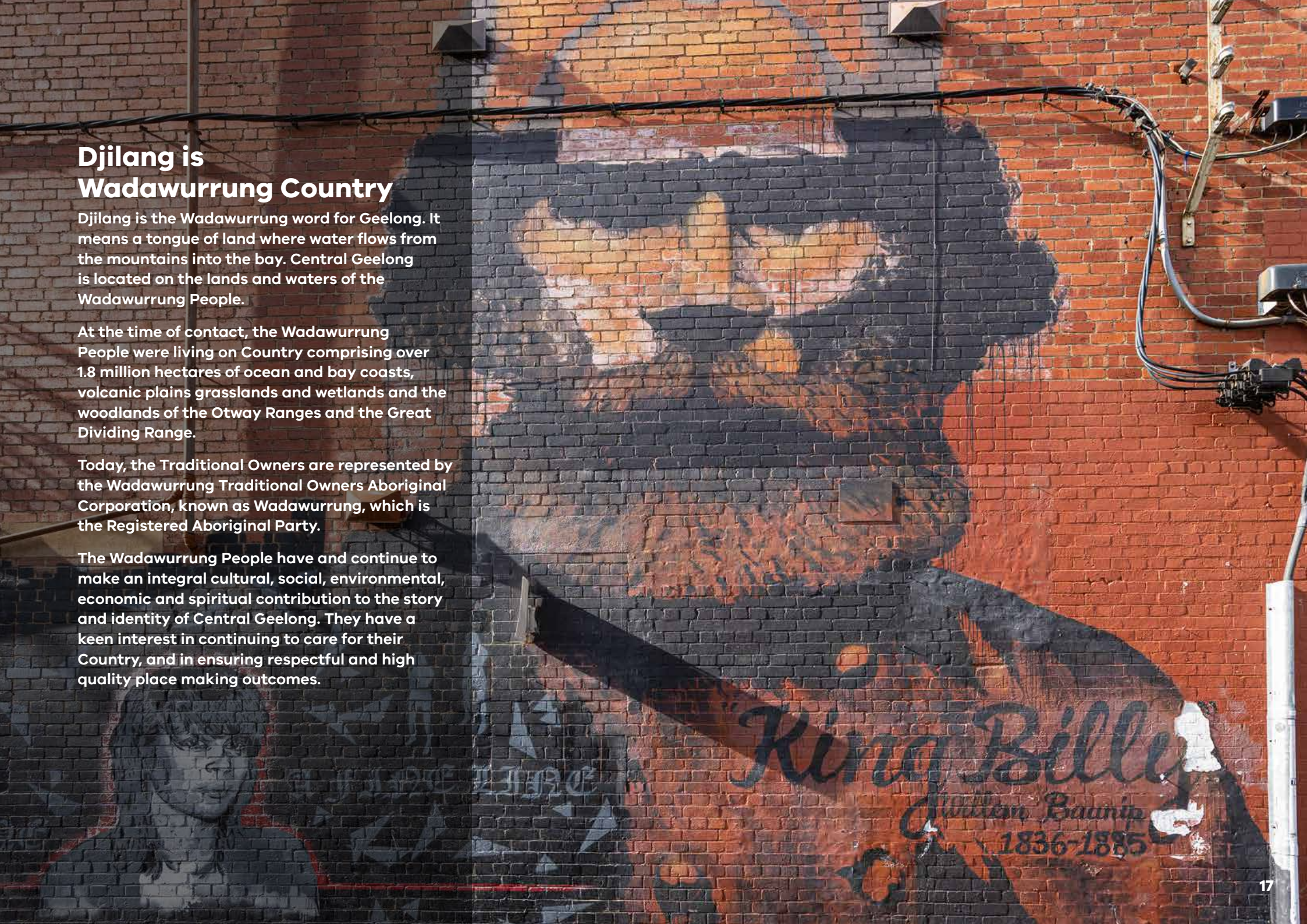
## Djilang is Wadawurrung Country

Djilang is the Wadawurrung word for Geelong. It means a tongue of land where water flows from the mountains into the bay. Central Geelong is located on the lands and waters of the Wadawurrung People.

At the time of contact, the Wadawurrung People were living on Country comprising over 1.8 million hectares of ocean and bay coasts, volcanic plains grasslands and wetlands and the woodlands of the Otway Ranges and the Great Dividing Range.

Today, the Traditional Owners are represented by the Wadawurrung Traditional Owners Aboriginal Corporation, known as Wadawurrung, which is the Registered Aboriginal Party.

The Wadawurrung People have and continue to make an integral cultural, social, environmental, economic and spiritual contribution to the story and identity of Central Geelong. They have a keen interest in continuing to care for their Country, and in ensuring respectful and high quality place making outcomes.



# History of Central Geelong



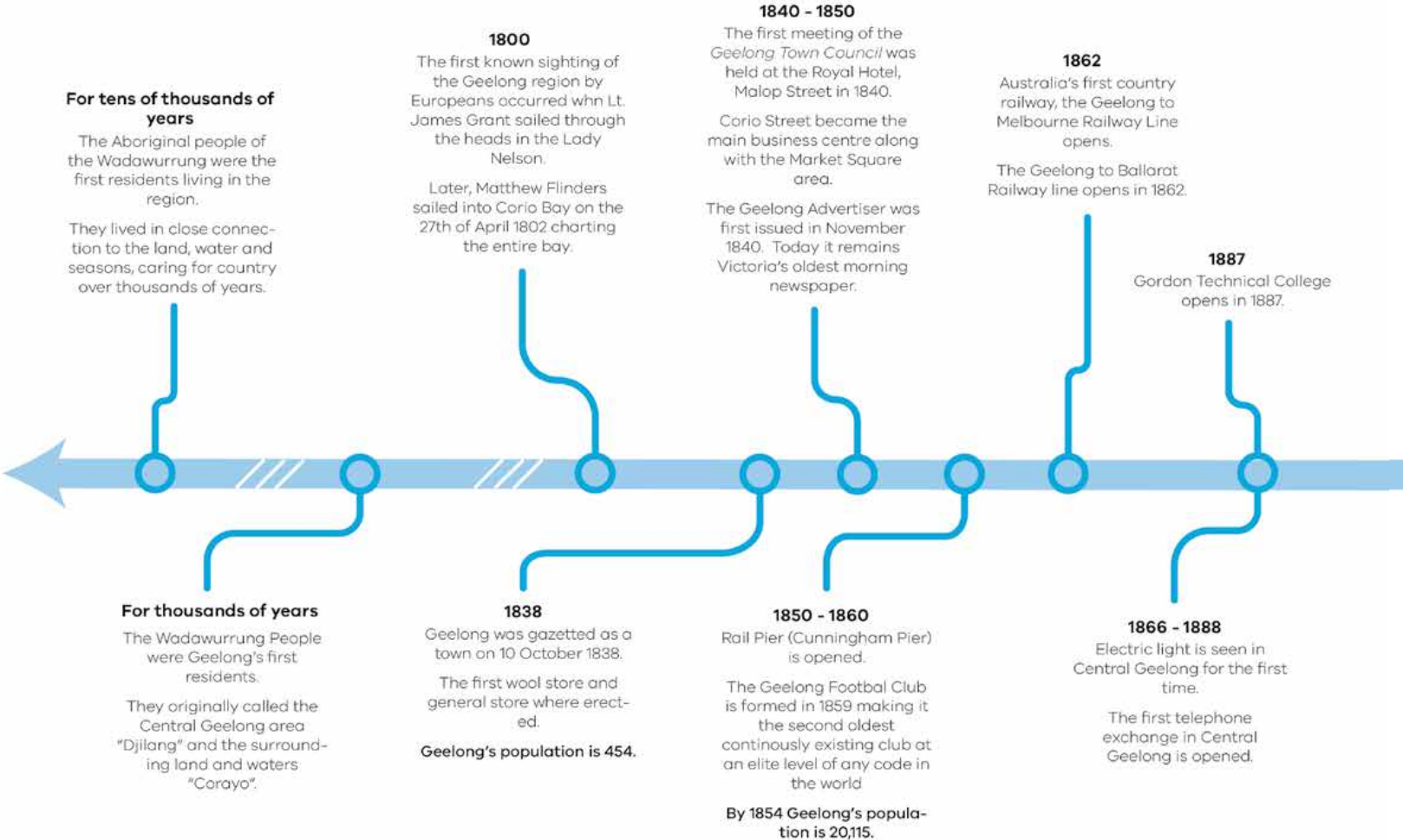
**Above:** Central Geelong looking north circa 1838.

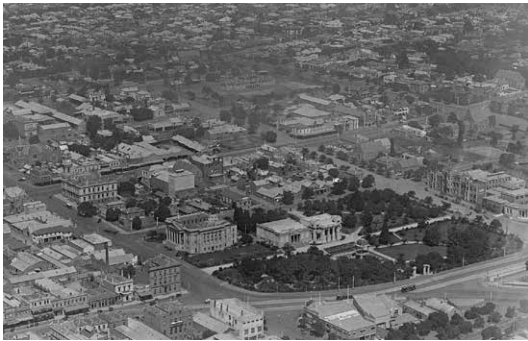
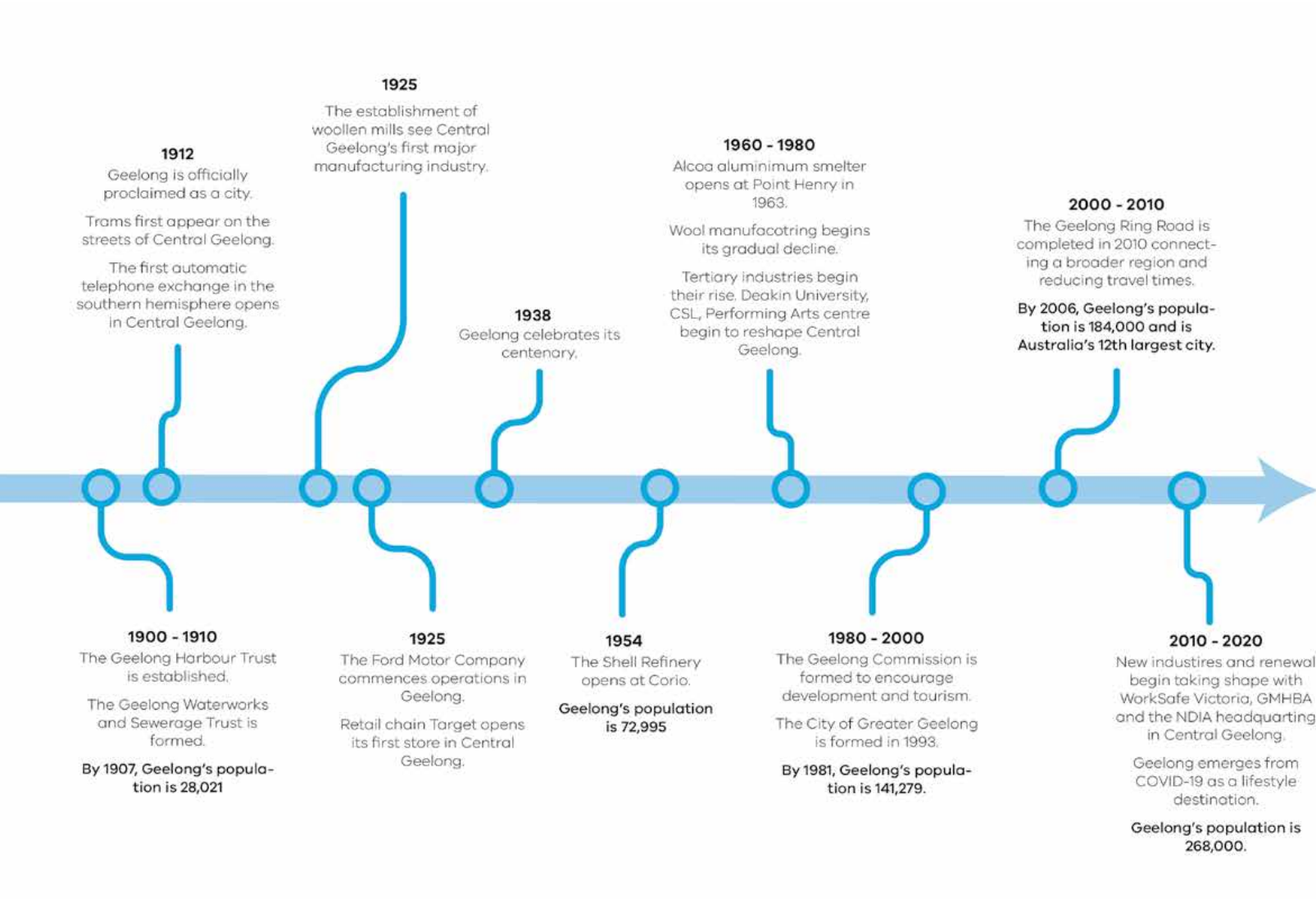


**Above:** Moorabool Street looking south circa 1860.



**Above:** Moorabool Street looking south circa 1900.





**Above:** Geelong Town Hall and Johnstone Park circa 1925.



**Above:** Geelong foreshore looking south east circa 1930



**Above:** Central Geelong looking west down Ryrie Street circa 1954

Source: *About Coryco: A thematic History of Greater Geelong, City of Greater Geelong, City of Greater Geelong*



## Central Geelong today

### Key views and vistas

- Views of Corio Bay down north-south streets
- Views of the Central Geelong skyline from the waterfront and from Corio Bay
- Distant views of the Central Geelong skyline from Point Henry, the Bellarine Peninsula, Belmont, Highton and the northern banks of Corio Bay

### Visitor attractions

- National Wool Museum
- Geelong Arts Centre
- Geelong Art Gallery
- Cunningham Pier
- Geelong Harbour and Marina

### Land use

- Retail hub for the local and regional community
- Geelong Hospital, St John of God Hospital
- Deakin University, Gordon Institute of TAFE
- WorkSafe Headquarters, NDIA Headquarters, GMHBA Headquarters
- Westfield Shopping Centre, Marketsquare Shopping Centre





## Street network

- Geelong Train Station
- Malop Street Green Spine
- Little Malop Street
- Western/Eastern Beach Road
- Moorabool Street

## Open space

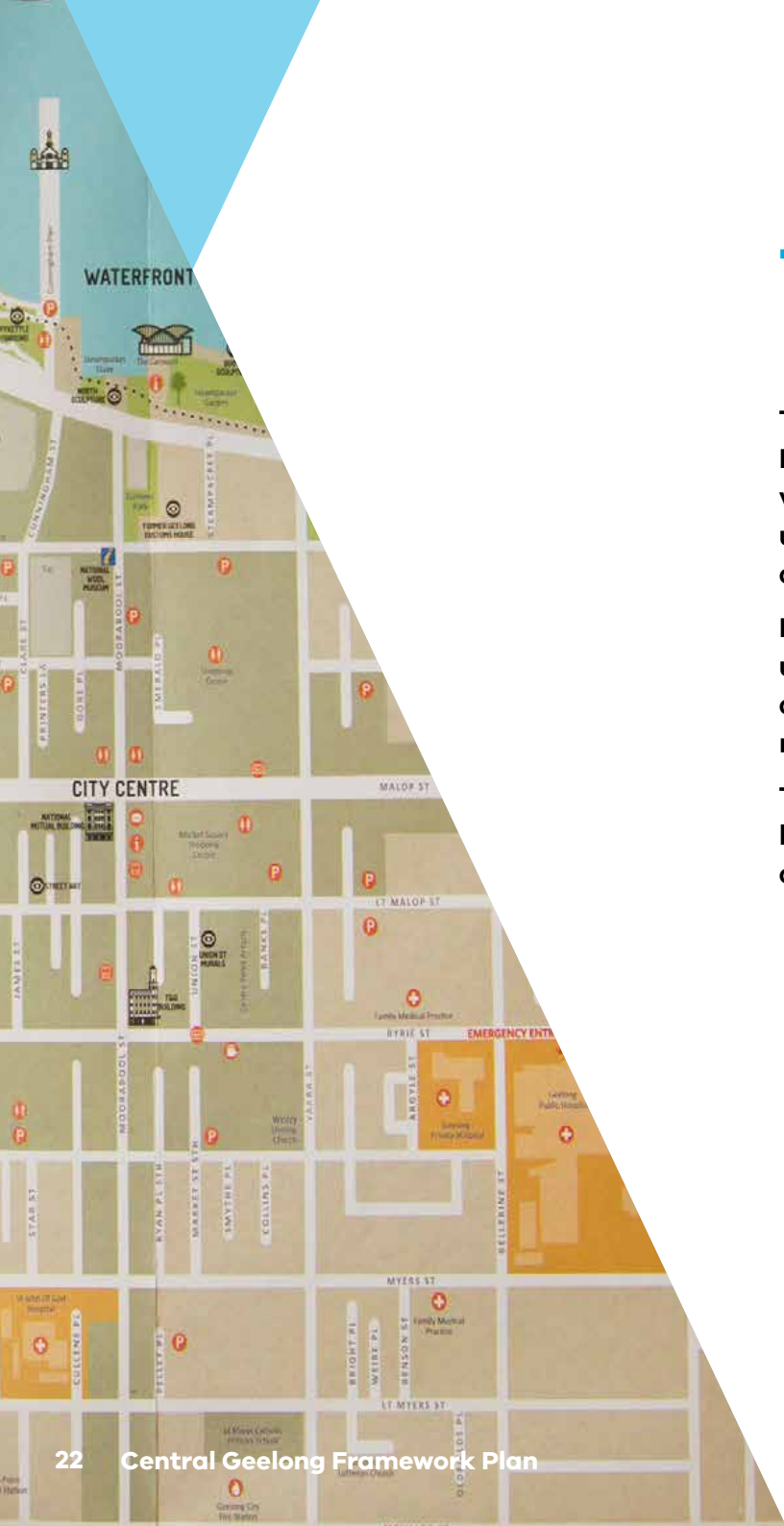
- Austin Park
- Johnstone Park
- Transvaal Square
- Steampacket Gardens
- Geelong foreshore and Corio Bay
- Eastern Beach



## Heritage and urban fabric

- Various churches and places of assembly
- Various woolstore and industrial buildings
- Geelong City Hall, Geelong Customs House and other civic buildings
- Heritage and retail streetscapes
- Gordon Institute of TAFE and other institutional buildings.





# This Framework Plan

**This Framework Plan comprises a Structure Plan and Urban Design Guidelines that sets a vision, presents strategic objectives grouped under four themes to guide use and development of Central Geelong to 2050.**

**It has been informed by inputs including an urban design assessment, land use demand and supply analysis, and a transport and movement analysis.**

**The community and a range of stakeholders have been consulted at several stages of developing this Framework Plan.**

The Framework Plan comprises:

- a Structure Plan that sets out the vision and objectives, strategies and actions for Central Geelong and
- Urban Design Guidelines that provides the built form direction and requirements that supports the Structure Plan.

The Framework Plan has been informed by three supporting documents:

- an urban design assessment of existing and proposed built form controls, prepared by MGS Architects, which was integrated into this Framework Plan
- a land use and market floor space supply and demand analysis and assessment, prepared by SGS Economics and Planning, which is a separate document
- a transport and movement analysis, prepared by Movement & Place Consulting, which is a separate document.

The Framework Plan was also informed by a range of policies and strategies relevant to Central Geelong. A complete list can be found in *Appendix B: Reference documents*.

## Developing this Framework Plan

The development of this Framework Plan has been informed by engagement with the City of Greater Geelong, the Wadawurrung, community groups, land holders and a range of key stakeholders.

This process involved:

- engagement with community, property and peak body representatives, conducted by the *Revitalising Central Geelong* partnership to test the existing directions, and identify future opportunities for Central Geelong
- consulting and collaborating with interdepartmental project teams to test timeframes and align objectives and strategies
- engaging with the *Geelong Authority* to identify opportunities for Central Geelong and where expectations could be clarified to inform decision-making about planning permit applications
- directly engaging with key stakeholders in a number of workshops, which provided valuable insights into stakeholders' current and future visions for Central Geelong
- directly engaging with key landowners and developers through group workshops to discuss and determine viable direction and controls
- public consultation on the draft Framework Plan and planning controls with 147 submissions received and reviewed
- review of submissions and advice provided by an independent Advisory Committee appointed by the former Minister for Planning, the Hon. Richard Wynne MP.



Figure 4. Framework Plan project timeline

# Transforming Central Geelong

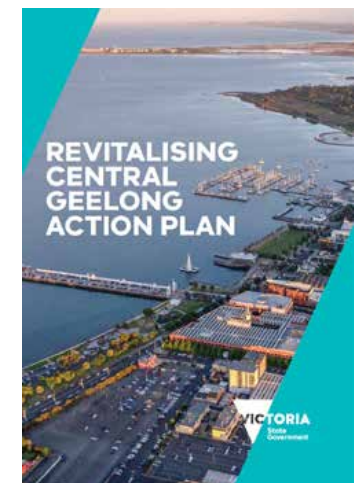
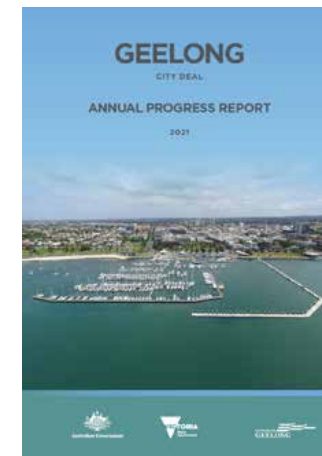
Central Geelong has grown rapidly exceeding the goals and vision of the *Central Geelong Structure Plan 2007*.

Government investment in Geelong and the region is unlocking its potential. Through the *Geelong City Deal 2017*, the Australian and Victorian governments, together with the City of Greater Geelong, will deliver \$502.48 million in investment to the region, supporting Geelong's continued economic diversification, the growth of the visitor economy and a thriving city centre.

The *Revitalising Central Geelong Action Plan 2016* sets out the Government's plan to drive change in central Geelong. The Revitalising Central Geelong Action Plan is designed to encourage:

- People living and working in the City
- Permanent and construction job creation
- Demand for retail and services
- Creativity and vibrancy across the city, day and night
- New development opportunities
- Private sector confidence to invest in Central Geelong.

As progress is made, the *Revitalising Central Geelong Action Plan* will continue to be refreshed and updated to support the growth and development outcomes for Central Geelong.





# Opportunities and challenges

## Building on investment

Since the original Geelong Structure Plan in 2007, there has been much investment in health, education, retail, cultural and civic activity. Central Geelong continues to attract new headquarters and has a growing housing market, evolving trends in retail and a vibrant hospitality scene.

All levels of government have invested in civic and infrastructure works, including the Malop Street Green Spine, the Geelong Arts Centre, upgrades to regional rail services and the future Geelong Convention and Exhibition Centre. All these contribute to a growing, vibrant Central Geelong.

To further facilitate investment and direct growth, an integrated planning approach is needed that focuses on the city's form and function and and strengths.

## Integrating a Wadawurrung identity

The Wadawurrung People know that if we care for Country, it will care for us. For tens of thousands of years, they have managed, cultivated and cared for the landscape where our towns and cities were established and continue to grow and prosper.

While the names of Geelong and Moorabool, Gheringhap, Yarra and Corio streets are derived from the Wadawurrung language, Geelong's cultural history and the enduring engagement of the Wadawurrung people with Geelong are not evident in Central Geelong's built form and open space.

There is a significant opportunity to help Aboriginal and non-Aboriginal communities to share knowledge about places of cultural and heritage significance and to understand how we can all work to respect and protect sensitive sites and strengthen Aboriginal cultural heritage.

## Building existing urban character

As buildings begin to grow in height and scale, it is essential to protect access to sunlight to the street and the open space network: these are key aspects of the amenity of the public realm.

Central Geelong has many individually significant buildings and heritage precincts. These have in places a fine-grain allotment, which is most evident in the retail core, which is an important part of the city's identity and character, which in turn support unique opportunities such as the emerging laneway culture.

There is a real risk that future growth and development will erode key elements of Central Geelong's urban fabric through the effects of inappropriate demolition or façadism and overly dominant building form and scale that overshadows and does not respect Central Geelong's character and heritage fabric.

## Respecting Central Geelong's natural setting and views

Central Geelong is bounded by Corio Bay to the north and the Barwon River to the south, with its topography forming an amphitheatre facing the bay. Church spires are traditional landmarks in Central Geelong, and buildings higher than seven storeys stand out against the skyline. Managing building heights within Central Geelong is therefore a fundamental consideration. Tall buildings must therefore be well designed and located to contribute positively to an emerging skyline.

From within Central Geelong, public views down many north-south streets to Corio Bay must remain integral to Central Geelong's sense of place and the experience of being in it. For example, the skybridge above Yarra Street provides a poor example as it interrupts key views to Corio Bay which is important to the Central Geelong experience.

## Protecting the public realm

Major public open spaces such as the waterfront and Johnstone Park are key recreation areas and major destinations, underpinning Geelong's identity as a lifestyle city. Geelong's gridded network of streets and laneways provides the potential for increased walking and cycling. Increased investment in amenity improvements in the public realm (such as in the Little Malop hospitality precinct and the Malop Street Green Spine) will provide high-quality open spaces.

However, the amenity of these public open spaces and key pedestrian paths risks degradation by overshadowing and stronger ground-level wind levels associated with new development, which would make the public environment unpleasant.

While Central Geelong's northern areas have ample open space, areas south of Ryrie Street have little to no open space, which is acknowledged by the City of Greater Geelong's open space strategy. It is also essential to provide and protect opportunities for new open space, as well as to enhance and protect the street environment. In this respect, there are opportunities to better utilise at-grade car parking sites owned by the council as open space.

## Improving access and mobility

Central Geelong benefits from a clearly defined network of wide streets and laneways, making it a very walkable environment and accessible within 20 minutes from virtually any point. While there have been significant efforts to improve the amenity of streets and laneways for walking and cycling, much of Central Geelong continues to be dominated by vehicular traffic.

As Central Geelong grows, it will be important to ensure that this walkability and the attractiveness of the public realm environment are enhanced and extended where possible with new pedestrian links. Crossovers and service access to future development must be carefully managed to improve, rather than erode, Central Geelong's walkability.

## Achieving sustainable growth and development

There is a need to support existing and future employers and industries in Geelong (such as health and government services) to locate in Central Geelong. Current built form controls are inadequate to support future land uses that will help Geelong grow and develop into a world-class city of design.

Most buildings in Central Geelong are between one and four storeys, and the overall building height and street wall are typically the same height. The retail core is mainly one or two storeys, with the taller woolstore buildings close to the waterfront reaching up to three to four storeys.

Current land use controls lack direction and specification. In and around the Geelong Train Station and the university area, high-density residential development has displaced more strategically important land uses such as those with a focus on commercial office and education purposes that enable long-term employment needed for Central Geelong.

Balancing the availability of commercial and residential floorspace will be a key challenge going forward. Job creation in Central Geelong is critical to supporting a diversified economy while new residents will bring vibrancy to Central Geelong.

## Ensuring design excellence

While Geelong does have some recently constructed, award-winning cultural buildings, recent office and high-density residential developments provide few examples of high-standard or excellent architecture.

Concerns include poor engagement with the heritage context and fabric, vehicular-dominated lower levels, including podium parking and poorly located crossovers, poor ground-floor interfaces that do not provide a good pedestrian experience and materials and glazing unsuited to the marine environment.

There is an opportunity to set not just base levels of design quality but to also provide guidance and direction to support design excellence through a range of different, more modern approaches.



# Managing growth and change

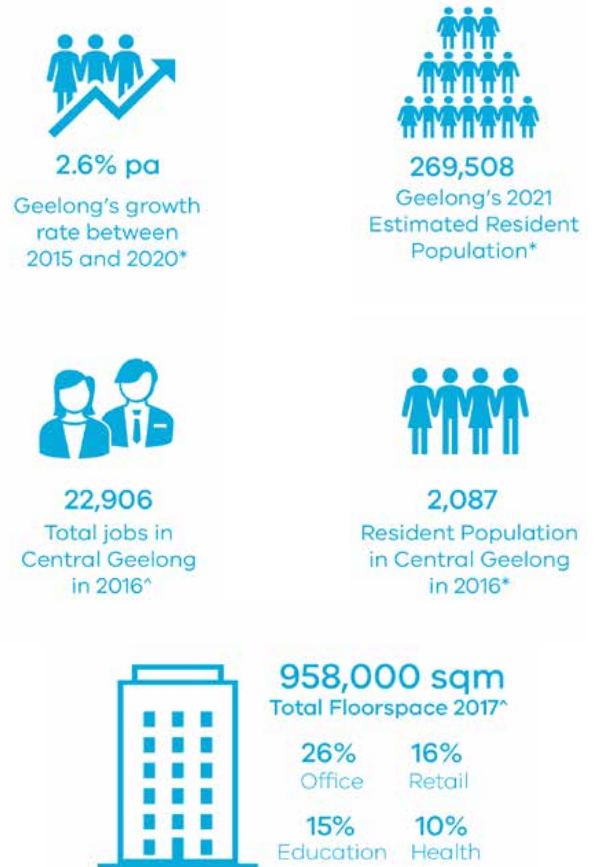
Greater Geelong's population has increased substantially in the past 10 years. With the highest population growth of all regional local government areas within Victoria, the influx of residents has shifted the focus of demand towards denser urban markets characterised by a greater variety of employment and education opportunities, central and diverse housing options and more recreation and lifestyle choices.

The Geelong region has seen strong population and housing growth in recent years, which is projected to continue. The Australian Bureau of Statistics estimates that in 2021, Geelong's population had reached 269,000 people.

Between 2011 and 2021, the City of Greater Geelong's population grew by about 53,000 people. *Victoria in Future 2019* estimates the city's population would continue to grow at a rate of 2% per annum to about 360,000 people by 2036, and 410,000 if that growth rate continues to 2050.

The *City of Greater Geelong Settlement Strategy 2020* estimates a population growth rate of 2.5% per annum which would see the municipality's population grow to about 387,000 people by 2036 and to 442,000 by 2050.

This means that over the next 30 years, the City of Greater Geelong's population could increase between 90,000 and 118,000 people by 2050.



Source:

\* Australian Bureau of Statistics

^ SGS Planning and Economics Small Area Land Use Projections model

## Increasing Central Geelong's share of the resident population

As at the 2021 census, Central Geelong's population is estimated to be around 2,500 people which is less than 1% share of the municipal population of Greater Geelong. Comparable regional cities such as Newcastle and Wollongong's share is 6.9% and 8.4% respectively of their municipal areas.

To create a modern and vibrant Central Geelong, an increase in the resident population is needed. Supporting sustainable population growth will in turn support and underpin demand of commercial floorspace and job creation as a result.

Population growth in Central Geelong needs to be carefully managed and balanced with the need to maintain its role as a regional centre and at minimum maintain, but preferably increase its share of municipal employment.

This Framework Plan should ensure that sufficient floorspace is provided to support the delivery of both residential and commercial land use to deliver long-term growth but more importantly, monitored to ensure that supply of residential floorspace does not unnecessarily compete with the need to provide employment generating commercial floorspace.

Table 1. Share of resident population in Central Geelong, 2011 to 2021

Share of resident population in Central Geelong, 2011 - 2021				
	2011	2016	2021	Average growth rate
Central Geelong	1,720	2,087	2,518	+3.1%
City of Greater Geelong <sup>^</sup>	210,875	233,429	271,057	+2.2%
Percentage Share in Central Geelong	0.8%	0.9%	0.9%	

Source: Australian Bureau of Statistics Census Data

## Maintaining Central Geelong's share of employment

Central Geelong is the main activity centre for the Greater Geelong municipal area and its broader region. One-fifth of all jobs within the City of Greater Geelong are located within the area covered by this Framework Plan.

Central Geelong has maintained a healthy average employment growth rate since 2011 and has maintained its overall percentage share of municipal jobs above 21%.

Central Geelong is sensitive to changes in regional and state economic trends and conditions and while growth has occurred, the growth has been concentrated in the office and hospitality sectors with decline witnessed within the retail sector.

Maintaining Central Geelong's overall municipal share of jobs is a key challenge. Providing sufficient commercial floorspace is critical to Central Geelong achieving jobs growth and maintain its role as a major regional employer. Protecting Central Geelong's retail offering and expanding commercial offerings through mixed use development forms part of this challenge.

Table 2. Share of jobs in Central Geelong, 2011 to 2016

Share of jobs in Central Geelong, 2011 to 2016			
	2011	2016	Average growth rate
Central Geelong	20,622	22,906	+2.1%
City of Greater Geelong	96,733	106,529	+1.9%
Percentage Share in Central Geelong	21.3%	21.5%	

Source: SGS Planning and Economics Small Area Land Use Projections model

## Land use and floor space trends and projections

This section summarises the land use demand and market assessment analysis for Central Geelong undertaken by SGS Economics and Planning which is set out in *Central Geelong Land Use and Market Assessment Update 2022*. Appendix C has more information about how floor space capacity and plot ratios were determined to inform this Framework Plan.

### Current floor space mix and trends

As of 2017, the *Census of Land Use and Employment* data for Central Geelong recorded around 958,000 square metres of employment and habitable floor space. Of this floor space:

- 29% is used for retail purposes
- 14% is used as office floor space
- 8% is used for residential and other types of accommodation.

In understanding current and future trends in supply and demand, SGS Economics and Planning observed the following.

#### Residential floor space

- Residential demand will continue to grow in Central Geelong, with apartment-style development beginning to play a stronger role in the overall housing composition.
- Factors that will continue to support apartment-style-led demand include decreasing household sizes, an ageing population and latent demand for diverse housing.
- The housing market will continue to be in direct competition with surrounding residential areas.

#### Office floor space

- There is an emerging office market, with over 50,000 square metres of A-Grade office space (more than 1,000 square metres) developed or coming onto the

market between 2020 and 2022, in addition to the 80,000 square metres approved between 2015 and 2019.

- Pre-pandemic, large, A-Grade office space is unlikely to come online without a significant level of pre-commitment of about 5,000 square metres.
- B-Grade office space (below 1,000 square metres) will continue to be in demand.
- Forecast population growth will support demand for population-driven services, including professional, business, health, personal and social services.

#### Retail and hospitality floor space

- Retail trade is one of Greater Geelong's largest employers, employing 10,836 people or 10.5% of the population in 2016<sup>1</sup>.
- Recent demand for retail floor space has been mixed. The number of retail establishments declined, but the number of accommodation and food services establishments increased since 2017.
- Forecast population growth across the Greater Geelong region and within Central Geelong will underpin increased demand for retail space.
- Retail property observers raised car parking and public transport connectivity as key issues.

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<sup>1</sup> *City of Greater Geelong Retail Strategy 2020–36 (August 2020)*.

## Providing future floor space

This Framework Plan and corresponding planning controls have been designed to deliver approximately 1.67 million square metres of new commercial and residential floors space (see Table 3). This will provide space for approximately 60,000 jobs and around 16,000 people by 2050.

This Framework Plan has considered the following component elements to determine the potential future floorspace needs for Central Geelong:

- Uses Greater Geelong (LGA) population growth rate of 2.5% a year consistent with the *City of Greater Geelong Settlement Strategy 2020*
- Estimates Central Geelong's LGA population percentage share as 2.5% and 3.0% in 2050
- Estimates Central Geelong's LGA employment percentage share as 22% in 2050

The methodology used to determine future floor space needs has taken into account an analysis of the existing market demands (pre-COVID-19), proposed infrastructure and other investments, and development aspirations for Central Geelong.

Employment projections were converted to floor space using floor space-to-job ratios to identify an employment floor space demand range.

Residential projections converted population to floor space by using Central Geelong's average household size combined with an assumed per-dwelling floor space to identify residential floor space.

## Considering COVID-19 impacts

The impacts of the COVID-19 pandemic are still unfolding, and it is too soon to assess medium or long term impacts on employment and residential floor space trends, which will likely be affected by a mix of negative and positive factors.

This Framework Plan and corresponding planning controls should be monitored overtime to determine whether growth and development aspirations are being achieved and whether the required floorspace is being achieved for both commercial and residential purposes.

Table 3. Central Geelong population and required floorspace to 2050

Population and required floorspace to 2050		
	Employment	Residential*
Total Population	60,000	13,000 to 16,000
Additional Floorspace	+967,000	+541,000 to 703,000

Source: Central Geelong land Use and Market Update (2022), SGS Planning and Economics

\* Dual figures account for projected share of LGA population in Central Geelong at 2.5% and at 3.0% in 2050

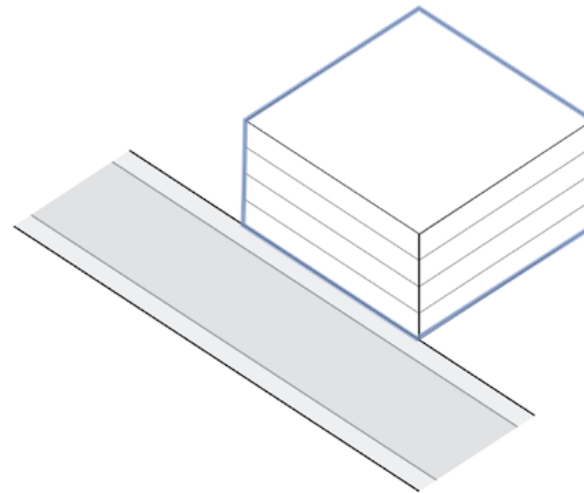
## Developing and applying plot ratios

This Framework Plan recommends discretionary plot ratios be applied to identified strategic development sites.

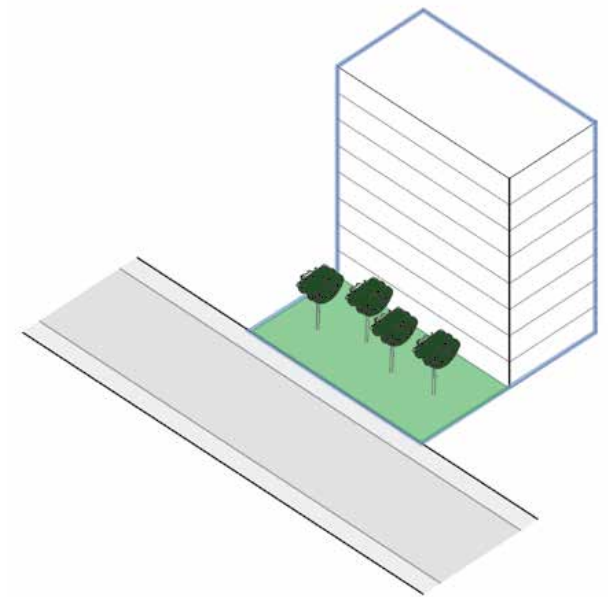
The plot ratio (also known as floor area ratio) is the ratio of a building's total floor space area to the area of the site it will be built on.

For example and as Figure 5. Plot ratio example shows, a plot ratio control of 4:1 allows a floor space area four times the land area. It would allow a four-storey building to be built over the entire site or an eight-storey building to be built on half the site. When combined with built form controls, plot ratio controls create a flexible framework within which many successful building forms can be achieved.

Site specific plot ratios and built form controls have been designed to respond to the unique spatial characteristics of each identified site to maximise the development outcome while supporting the Framework Plan's strategic vision and objectives.



Plot Ratio 4:1  
4 Storeys (100% site coverage)



Plot Ratio 4:1  
8 Storeys (50% site coverage)

Figure 5. Plot ratio example







An aerial photograph of a city waterfront, showing a marina with several boats, a parking lot, and various buildings. A large teal diagonal shape overlays the right side of the image, creating a modern, graphic design. The text is placed on the teal background.

# The Structure Plan

**This section sets out the vision and objectives, strategies and actions for Central Geelong to guide development through to 2050.**

# The vision

*“By 2050, Central Geelong is home to 16,000 people and 60,000 jobs. It is the heart of Djilang (Geelong), Victoria’s second-largest city, with strengths in health, services, education and events. People are connected to its waterfront, high-amenity lifestyle and clever, creative culture.”*



# The objectives

The vision on the previous page articulates the following four themes and objectives guiding this Framework Plan for Central Geelong.

## A thriving economy and job creation

1. Establish Central Geelong as a national, state and regional centre accommodating 60,000 jobs.
2. Maintain and enhance Central Geelong as a vibrant retail, hospitality, arts and cultural hub
3. Create diverse housing choices to accommodate 16,000 residents
4. Provide adequate social and affordable housing to meet the needs of a growing community
5. Strengthen Central Geelong's role as a clever, creative city and a destination for visitors and major events

## A connection to place

6. Ensure built form outcomes reflect each precinct's preferred character and amenity
7. Ensure built form is of an appropriate scale when viewed from its surrounds and from a distance
8. Master plan and coordinate the development of strategic development sites
9. Celebrate, protect and appropriately interpret Aboriginal cultural heritage
10. Retain, preserve and protect Central Geelong's heritage built form fabric and character

## Getting around with ease

11. Make Central Geelong more walkable and safer
12. Provide safe, direct and connected cycling routes
13. Refine and undertake phased uplifts to Central Geelong's public transport network
14. Develop the Geelong Station Precinct into a world-class transport hub
15. Strategically manage the provision of car parking

## A vibrant city life

16. Ensure buildings provide high-quality, interactive interfaces to the street
17. Protect existing and provide new high-quality, accessible open space
18. Provide high-quality streetscape environments
19. Provide social infrastructure and utilities to meet growing needs
20. Encourage sustainable development that responds to climate change

# A thriving economy and job creation

The Geelong region is experiencing unprecedented growth. To support this growth, Central Geelong's important role as the region's centre for health, education and retail services and its function as a key visitor attractor must be maintained and enhanced.

Forecast population growth will support demand for a range of population-driven services (such as professional, business, health and retail services). Central Geelong's emerging office market, while promising, faces an uncertain future due to the potential long-term impacts of COVID-19.

As businesses and workforces adapt to a post-pandemic environment, demand for large commercial office space could change. Central Geelong's recent reliance on the relocation of government services to increase commercial floor space output will need to evolve to include a range of private offers at different scales and price points.

To achieve 60,000 jobs and 16,000 residents by 2050, approximately 1.67 million square metres of new floor space will be needed within the proposed planning controls. This will provide for sufficient growth from the start of the Framework Plan and provide and cater for any positive post-pandemic effects that may result.

Careful management of supply and demand is needed to ensure that residential supply does not dominate and there are ample opportunities for commercial development to occur over the period of the Framework Plan.

## Issues

- Adequate capacity for residential and commercial floor space to meet demand.
- Diversity of housing that contributes to Geelong's social and affordable housing needs.
- Over-reliance on government-led demand for commercial floor space in the medium to long term.
- COVID-19 pandemic's direct impacts and changes to the way people work, socialise and spend.
- Central Geelong's low residential population and limited services to support growth.
- Low supply of short-term accommodation for business and tourism.
- Low supply of high-quality office space.

## Opportunities

- Provide and prioritise world-class liveability to attract people to Central Geelong.
- COVID-19 recovery could accelerate population growth which could stimulate commercial growth.
- Increase small-scale enterprise, multi-use spaces and education-to-employment transitions.
- Leverage population growth to deliver new jobs and services in Central Geelong.
- Provide floor space capacity within planning controls to accommodate growth.
- Recognise the capacity and contribution to the activity centre of adjacent mixed-use residential areas.
- Facilitate growth and leverage off existing institutions, headquarters, events and attractions.
- Retain and enhance active street frontages and laneways.
- Ensure land use controls are flexible in responding to changing needs and demand.
- Attract new regional government agency headquarters and greater private office development.



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## Objective 1. Establish Central Geelong as a national, state and regional centre accommodating 60,000 jobs

Central Geelong is home to government, corporations, businesses and institutions providing a range of goods, services and diverse employment for a growing economy and local and regional communities. The health and social assistance sectors are the dominant employers, accounting for over one-third of all jobs. Central Geelong is also the primary centre of retail activity, education and culture in the region.

Demand for employment-related floor space in Central Geelong is expected to be strong over the next 30 years, underpinned by continued investment in the health sector and Geelong's economic transition into a clever, creative economy. The number of jobs in Central Geelong could increase from 23,000 currently to about 60,000 by 2050. This growth would require and additional 967,000 square metres of employment-related floor space.

There are also opportunities to build on Central Geelong's emerging role as the location of the headquarters of major government entities. The Transport Accident Commission, WorkSafe Victoria, the National Disability Insurance Agency and GMHBA currently all call Central Geelong home.

The Knowledge and Enterprise Precinct provides many opportunities for businesses to establish regional hubs and headquarters in Geelong. The precinct is strategically located between the Retail Core and Station precincts, and it has strong connections with the Geelong waterfront, Deakin University and the future Geelong Convention and Exhibition Centre. To accommodate and attract future investment in new

regional hubs and headquarters, employment-generating uses should be prioritised in this precinct over residential and other non-job-creating uses and development.

*Activity Centre Zone Schedule 1 (ACZ1)* will be the primary planning control used to support Central Geelong's continued development. The ACZ1 will be expanded in the east, south-west and south-east, where there are mixes of land use zones, to form an area of about 200 hectares. Figures 6 and 7 show the existing and new extents of the ACZ1.

Expansion of the ACZ1 is the most appropriate planning control to cater for the mix of commercial and residential land use and development outcomes this Framework Plan envisages. Existing public land within the Public Use Zone Schedules 2 (Education) and 3 (Health) will remain.

There will be a new West Village Precinct in the south-west of the expanded ACZ1, with the intention of achieving mixed-use outcomes strongly focused on higher-density urban living. An area in the south-east will be included in a new Health Precinct, emphasising the area's contribution to the health and medical facilities located there and providing increased residential densities. The ACZ1 will also be extended to the east side of Bellerine Street to ensure a consistent approach to the mainly retail-facing frontages.



## Strategies

- Provide an additional 967,000 square metres of commercial floor space capacity to provide space for 60,000 jobs by 2020.
- Protect Central Geelong’s role as the primary location for office development for the region.
- Prioritise commercial floor space in the Knowledge and Enterprise Precinct to support job creation and retain the capacity for national, state and regional headquarters.
- Support Central Geelong’s role as a vocational education and training hub for the region, prioritising education in the Knowledge and Enterprise Precinct and the Cultural Precinct.
- Expand the Activity Centre Zone to:
  - the east of Bellerine Street between Corio Street and Ryrie Street to include existing retail and commercial areas
  - the south-west to Mckillop Street and Latrobe Terrace to provide a mixed-use residential area
  - the south-east bordered by Mckillop Street and Swanston Street to expand and provide for a mixed-use Health Precinct.

Actions		Timeframe	Lead/partner
Action 1.	Expand the ACZ1 to include additional areas identified in Figure 7. Central Geelong Activity Centre Zone’s new extent.	Immediate	DTP
Action 2.	Ensure a sufficient supply of commercial floor space commensurate with the upper limit of the Optimistic scenario through appropriate built form controls in the ACZ1.	Immediate	DTP
Action 3.	Actively monitor commercial floor space by: <ul style="list-style-type: none"> <li>• developing an appropriate methodology and monitoring approach for Central Geelong</li> <li>• undertake active monitoring on a regular basis determined by the methodology.</li> </ul>	Ongoing	DTP



Figure 6. Central Geelong Activity Centre Zone's previous extent



Figure 7. Central Geelong Activity Centre Zone's new extent



## Objective 2. Maintain and enhance Central Geelong as a vibrant retail, hospitality, arts and cultural hub

The *City of Greater Geelong Retail Strategy 2020–36* defines a hierarchy of retail centres for Greater Geelong. Central Geelong will continue to be the regional centre, but its role is expected to change over time due to greater competition from other centres, growth in online retailing and opportunities to service a growing local catchment.

Central Geelong will need to create a point of difference from other centres, including by diversifying uses and the experiences it offers (such as arts and culture, entertainment, hospitality and event experiences).

The long-term directions of major retail sites will result from a detailed master planning process led by landowners and involving all levels of government and the wider community. There are opportunities to better integrate enclosed shopping centres with the rest of the city through improved connectivity and the activation of street frontages, and opportunities for a greater diversity of uses and experiences.

The city block that encompasses Market Square, including the area relating to Little Malop Street, has great potential. This area has historically been recognised as the centre of Central Geelong, and the block was once an important marketplace. As Central Geelong grows, this area can be transformed into a place with a thriving laneway culture, given the success of Little Malop Central.

Demand for high-quality accommodation will grow with the expansion of employment, education, health and major events in Central Geelong. The *Greater Geelong and The Bellarine Tourism Development Plan 2019–22* forecast a shortage of over 300 serviced apartments and over 300 premium hotel rooms in the Geelong urban region. More accommodation in Central Geelong will help address the estimated regional shortage of about 2,400 rooms the plan identified.

### Strategies

- Protect the role of Central Geelong as the region's primary retail centre, including by prioritising shops, food and drink premises and nightclubs in the Retail Core Precinct to maintain a vibrant city centre.
- Provide flexibility for ground-floor land uses to adapt to changing economic circumstances over time and so minimise vacancies.
- Monitor the supply of and demand for short-term visitor accommodation to ensure it meets the needs of visitors and industry.
- Increase the range and quality of short-term visitor accommodation.

Actions	Timeframe	Lead/partner
Action 4. Design and deliver a master plan that incorporates the city block containing Market Square and Little Malop Central in consultation with landholders.	Short	CoGG

## Objective 3. Create diverse housing choices to accommodate 16,000 residents

Growth in the resident population will unlock Central Geelong’s economic potential. More people living in Central Geelong will mean a more vibrant, active and attractive environment for people in the city centre.

Mixed-use development will leverage Geelong’s transit-rich environment to deliver diverse housing typologies, new workplaces and community facilities. This will contribute to places that are accessible, safe and diverse for people of all abilities, ages and cultures.

City dwellers seek the vibrancy of downtown living rich in activities, well-serviced and close to jobs, schools and public amenities. Central Geelong is well-positioned to provide housing that meets those requirements as a national, state and regional hub and is a suitable location for affordable and social housing.

The resident population is projected to grow to between 7,000 and 16,000 people by 2050, requiring between 2,800 and 5,500 additional dwellings. Most of this new housing in Central Geelong is expected to be delivered across a range of residential building types, including shop tops and apartment-style developments utilising a variety of building form typologies.

This Framework Plan’s objectives and strategies aim to ensure Central Geelong is a ‘living city’, offering housing choices that help develop a uniquely Geelong character. As more people choose to call Central Geelong home, providing a diversity of housing across different built form typologies will allow more choices. The City of Greater Geelong’s proposed Living 3220 initiative will seek to facilitate housing development in Central Geelong that aligns with the desired housing mix by analysing residential market needs and understanding barriers to development and identifying opportunities to encourage choice and diversity to support the future resident population.

### Strategies

- Provide an additional 700,000 square metres of new residential floor space to support a resident population of 16,000 people by 2050.
- Provide for a diverse range of residential and accommodation types and sizes in strategic locations.
- Prioritise residential and accommodation uses in the Station, West Village, York and Bayview Village precincts.
- Facilitate student accommodation for domestic, interstate and international students, prioritising the Station and West Village precincts.

Actions	Timeframe	Lead/partner
Action 5. Develop the Living 3220 initiative to support greater choice and diversity of housing within Central Geelong.	Short	CoGG
Action 6. Ensure a sufficient supply of residential floor space commensurate with the upper limit of the Optimistic scenario through appropriate built form controls in the ACZ1.	Immediate	DTP
Action 7. Actively monitor residential floor space by: <ul style="list-style-type: none"> <li>• developing an appropriate methodology and monitoring approach for Central Geelong</li> <li>• undertaking active monitoring on a regular basis determined by the methodology.</li> </ul>	Ongoing	DTP

## Objective 4. Provide adequate social and affordable housing to meet the needs of a growing community

The *City of Greater Geelong Social Housing Plan 2020–41* notes there are only 3,300 social housing dwellings in Geelong, compared to an estimated 7,200 Geelong households that urgently need social housing. The plan also notes that about 6,400 people are living in the private rental market and paying more than 30% of their income on rent, and often more than 50%. The plan estimates that the total demand for social housing is about 10,500 dwellings, or about 11% of all households. A further 13,500 new social housing dwellings are needed by 2041.

The plan proposes the city take a leadership role in advocating for an increase in social housing and that it invests in social housing, including by:

- contributing council-owned land surplus to its requirements
- determining a modest annual budget
- negotiating funding agreements with the State and Commonwealth governments
- establishing an affordable housing trust and an affordable housing company.

Social and affordable housing targets for Central

Geelong should be considered within the wider context of Greater Geelong’s needs. Any targets developed should consider the types of housing needed and its location, and a strategic approach should be taken to provision across the municipality, with Central Geelong providing its fair share.

There are many approaches and models for Geelong to consider, including the Fishermans Bend and Arden Street renewal projects, to ensure different types of development make appropriate contributions. Any approach developed must also consider Geelong as a regional city and be subject to consultation before being implemented into the Greater Geelong Planning Scheme.

### Strategies

- Develop and apply appropriate requirements to deliver social and affordable housing needs in Central Geelong as part of a municipal approach.
- Ensure the approach to providing social and affordable housing considers the *City of Greater Geelong Social Housing Plan 2020–41*.

Actions	Timeframe	Lead/partner
Action 8. Identify appropriate policy and development contributions for social and affordable housing in Central Geelong, having regard to the <i>City of Greater Geelong Social Housing Plan 2020–41</i> , and implement them through the Greater Geelong Planning Scheme.	Short	CoGG

## Objective 5. Strengthen Central Geelong's role as a clever, creative city and a destination for visitors and major events

Central Geelong is developing into a major night-time destination. There are opportunities to increase the diversity of night-time activities; encourage the activation of streets, laneways and public spaces across extended hours; and leverage major events.

The regeneration of Little Malop Central into a thriving restaurant and café precinct demonstrates the potential of Central Geelong's historic laneways. The *Laneways Action Plan 2018* identifies a priority network of laneways to improve cross-city linkages and create economic opportunities.

A clever, creative city with the size and vibrancy of Geelong requires cultural assets that meet the community's growing, changing needs. Central Geelong is at the heart of the city's flourishing arts scene, and major investments in recent years have included the completion of the Geelong Library and Heritage Centre, which sets the standard for the future of libraries, and the redevelopment of the Geelong Arts Centre. The *Arts and Cultural Precinct Masterplan 2017* sets out the next stages in the precinct's development, including public realm and streetscape upgrades to create a distinctive character and opportunities for street-based cultural events and installations.

Geelong is well-positioned to cater to the lucrative domestic and international markets for business events and conferences. The Victorian Government's commitment to delivering the Geelong Convention and Exhibition Centre will cement Geelong's standing as Australia's leading regional centre for business events.

As Central Geelong's population grows, conflicts between residential and commercial uses are inevitable, mainly around the impacts of live music and after-hours entertainment. The *Central Geelong Live Music Action Plan 2017* estimates that live music, directly and indirectly, contributes an estimated \$150 million or more annually to Geelong's economy. The 'agent of change' planning principle assigns responsibility for noise attenuation measures to a new use or development being introduced into an existing environment. This means new residential development should consider including noise attenuation measures in their designs when located close to an existing, emerging or planned hospitality venue or area (such as in the Knowledge and Enterprise, Bayview Village and Retail Core precincts).

University Hospital and St John of God Hospital are at the core of Geelong's growing medical and health facilities and services, and various supporting and allied services support these hospitals directly and indirectly. The Health Precinct will continue to support mixed-used outcomes, ensuring that existing and new medical and health services in this area continue to flourish.

**Top right** The Cadel Evans Great Ocean Road Race is an annual professional one-day road bicycle racing for both men and women starting and finishing in Geelong.

**Bottom right:** The capacity of Geelong Arts Centre is being increased with a \$140m investment providing multiple new performance venues, an upgraded box office and refurbished back-of-house and administration facilities.





## Strategies

- Provide a streetscape environment that is safe and attractive at all times of the day and night.
- Support the growth of a night-time economy and a thriving music scene where residential amenity can be reasonably managed through the agent of change principle.
- Activate Central Geelong’s laneways and provide new commercial opportunities.
- Continue to work with delivery partners to implement the Arts and Cultural Precinct Masterplan.
- Facilitate opportunities to access commercial spaces for short-term, pop-up creative activities and boutique retail shops.
- Promote Central Geelong as the region’s arts and cultural heart, with the Cultural Precinct being the primary location for arts and cultural facilities.
- Support Central Geelong as the region’s medical and health services hub, prioritising health, medical and related services, businesses, education and research in the Health Precinct.
- Support Geelong’s waterfront as a destination for recreation, tourism, and major events, including complementary commercial uses, facilities and infrastructure.

Actions	Timeframe	Lead/partner
Action 9. Continue to work with delivery partners to implement the <i>Laneways Action Plan 2018</i> .	Ongoing	DTP CoGG
Action 10. Implement the <i>Central Geelong Arts and Cultural Precinct Masterplan</i> .	Short	DTP CoGG
Action 11. Deliver a world-class (in both design and function) Geelong Convention and Exhibition Centre.	Short	RDV
Action 12. Implement the <i>Central Geelong Live Music Action Plan</i> .	Ongoing	CoGG



## Geelong Convention and Exhibition Centre

The Geelong Convention and Exhibition Centre (GCEC) forms the centrepiece project of the Geelong City Deal. Under the deal, a total of \$294 million has been allocated to the project, with contributions from all levels of government.

The project will be delivered on the 1.6 hectare former Deakin University site, which enjoys uninterrupted views across Corio Bay.

The GCEC facility will include a convention and exhibition facility with a 1,000+ seat plenary, and at least 3,700 square metres of multi-purpose space that can be configured to incorporate exhibition halls, breakout rooms, a banquet room and other complementary facilities.

In addition to the GCEC facility, the project will deliver an integrated precinct development that activates the entire site and includes a privately funded 200+ room 4 star hotel and other complementary commercial (including food & beverage and retail offerings).

The project will support 600 jobs during construction and an additional 270 ongoing jobs once complete. The hotel will further boost employment opportunities and strengthen the region's visitor economy.

*Image: Artist impression of the Geelong Convention and Exhibition Centre foyer (Development Victoria)*



# A connection to place

Central Geelong has a natural waterfront setting and history, enjoying views of Corio Bay and Wurdi Youang (the You Yangs) beyond. At street level, there is an openness to the sky set within a rich tapestry of heritage buildings within a clearly framed, orderly network of streets and laneways.

Today, Central Geelong has a low-rise, low-density city form. Projected growth means that the building height, scale and intensity of use will increase. Central Geelong's sense of place, character, identity and appeal must be maintained, while growth is supported and change managed.

High-quality, well-integrated and well-designed buildings will be important for Central Geelong's success. Great urban design will shape Central Geelong's built environment, improve its liveability for the community and businesses.

Urban design (the process of designing and shaping the physical features of cities) is a long-term process that reflects decisions about specific projects in a dynamic, evolving urban environment. This Framework Plan's place-based approach identifies seven precincts that will include a variety of building types, heights and densities that fit well within their surrounding context while also helping create a new, distinctive character for Central Geelong.

A high-quality, connected public realm will bring together the precincts and connect Central Geelong with neighbouring areas to help attract new businesses and residents. Its streets and spaces will be framed by high-quality architecture and design excellence to continue a legacy of exemplary urban design.

## Issues

- Lack of design guidelines that reflects the city's context, values and vision.
- Inconsistent hierarchy of height limits across Central Geelong, particularly surrounding Mercer Street.
- Poor guidance about preserving views to Corio Bay.
- Lack of protection from overdevelopment that reduces daylight access and blocks views.
- Lack of design direction and protection measures resulting in risks to the existing and future amenity of the city's precincts, streets and laneways.
- Lack of identifiable destination points and features on arrival at Central Geelong, notably in the Station Precinct.
- Poor precinct identity and integration with Central Geelong.
- Inadequate planning controls to manage development heights, setbacks and built form for precincts and all Central Geelong.
- Inconsistent intensity and integration of development with surround development.

## Opportunities

- Ensure the use and development of land supports preferred land uses and the urban characters of Central Geelong and its emerging precincts.
- Support the Wadawurrung People to share their knowledge about Country and to understand how we can all work to respect and protect sensitive sites and strengthen Aboriginal cultural values.
- Maintain Geelong's status as a UNESCO City of Design and an attractive lifestyle city by ensuring new development is sustainable, meets a high design quality standards.
- Maintain and enhance Central Geelong's inherent walkability with good design outcomes that support its network of streets, laneways and open spaces.
- Ensure design excellence resulting in interesting, high-quality buildings that improve the urban environment, contribute to its amenity and character and add to city pride.
- Ensure built form reinforces a human scale at ground level, presenting a pleasant streetscape rhythm and interaction with the street that makes for an attractive, engaging environment.
- Protect and respect Central Geelong's many historic buildings and places of cultural value, contributing to the fabric and character of its environment.
- Protect access to daylight for all public open spaces and streets to contribute to a high-amenity environment.



## Objective 6. Ensure built form outcomes reflect each precinct's preferred character and amenity

Central Geelong has an incredible natural setting, framed by the Barwon River and Corio Bay. It benefits from its clear urban structure of streets and lanes, significant heritage parks and buildings, a picturesque foreshore and fine-grain heritage buildings.

Central Geelong has the potential to leverage its strengths as a UNESCO City of Design and attractive lifestyle city by ensuring new development is sustainable, meets design quality standards and builds on rather than erodes these strengths.

The City of Design designation is about embracing a design mindset, nurturing creative talent and fostering a culture of design to build a more sustainable, resilient and inclusive community. It fits perfectly with Geelong's 30-year, community-led vision to be internationally recognised as a clever, creative city and region.

All new buildings should aspire to design excellence and innovation, consistent with the City of Design designation. Architectural quality and design excellence should be achieved in part through the effective use of high-quality materials, innovative and sustainable building design and construction methods and sensitive, thoughtful responses to a development's surroundings.

### Strategies

- Street wall heights and setbacks will reinforce the framework of the street grid and create an engaging, relatable scale and character.
- Retain fine-grain allotment patterns and heritage fabrics through lower preferred heights in key character areas and upper setbacks that retain the three-dimensional form of heritage buildings beyond the facade.
- Maintain key views along all north-south streets to ensure unobstructed views of the sky and Corio Bay.
- Ensure tall buildings positively contribute to Central Geelong's skyline when viewed from Corio Bay and neighbouring suburbs to the east, south and west.
- Encourage street walls and upper façades that provide vertical rhythm, height variety and architectural expression.
- Design buildings to endure, with all materials, glazing systems and finishes suitably robust and durable for a coastal environment.
- Incorporate best practice sustainability principles into new buildings to improve their liveability and reduce their overall environmental impacts.

Actions	Timeframe	Lead/partner
Action 13. Apply built form controls as identified in the Urban Design Guidelines section of this Framework Plan as part of an amendment to the ACZ1.	Immediate	DTP



## Geelong, a UNESCO City of Design

Geelong has a long, proud history as a creative city and a design leader.

In 2017, Geelong was designated a member of the UNESCO Creative Cities Network.

The City of Design designation is about using creativity and innovation to build more sustainable, resilient and inclusive communities.



## Objective 7. Ensure built form is of an appropriate scale when viewed from its surrounds and from a distance

Central Geelong provides a focal point that gives Geelong its visual identity. Central Geelong is visually prominent from several surrounding regional vantage points, including Corio Bay’s foreshore and waters, elevated areas within urban Geelong and approaches to the Bellarine Peninsula. These and other locations present a visual entrance and identity as a waterfront city. Views from Corio Bay provide postcard imagery that projects Geelong beyond regional and state boundaries to the world.

Five preferred maximum building heights, shown in *Figure 8. Preferred maximum building heights*, have been identified to promote a range of building types to support diversity in the appearance and scale of buildings and to support the land use vision for each precinct while responding to Central Geelong’s valued heritage attributes and streetscapes. The preferred maximum building heights respond to the urban context and development potential of sites, and they have been informed by:

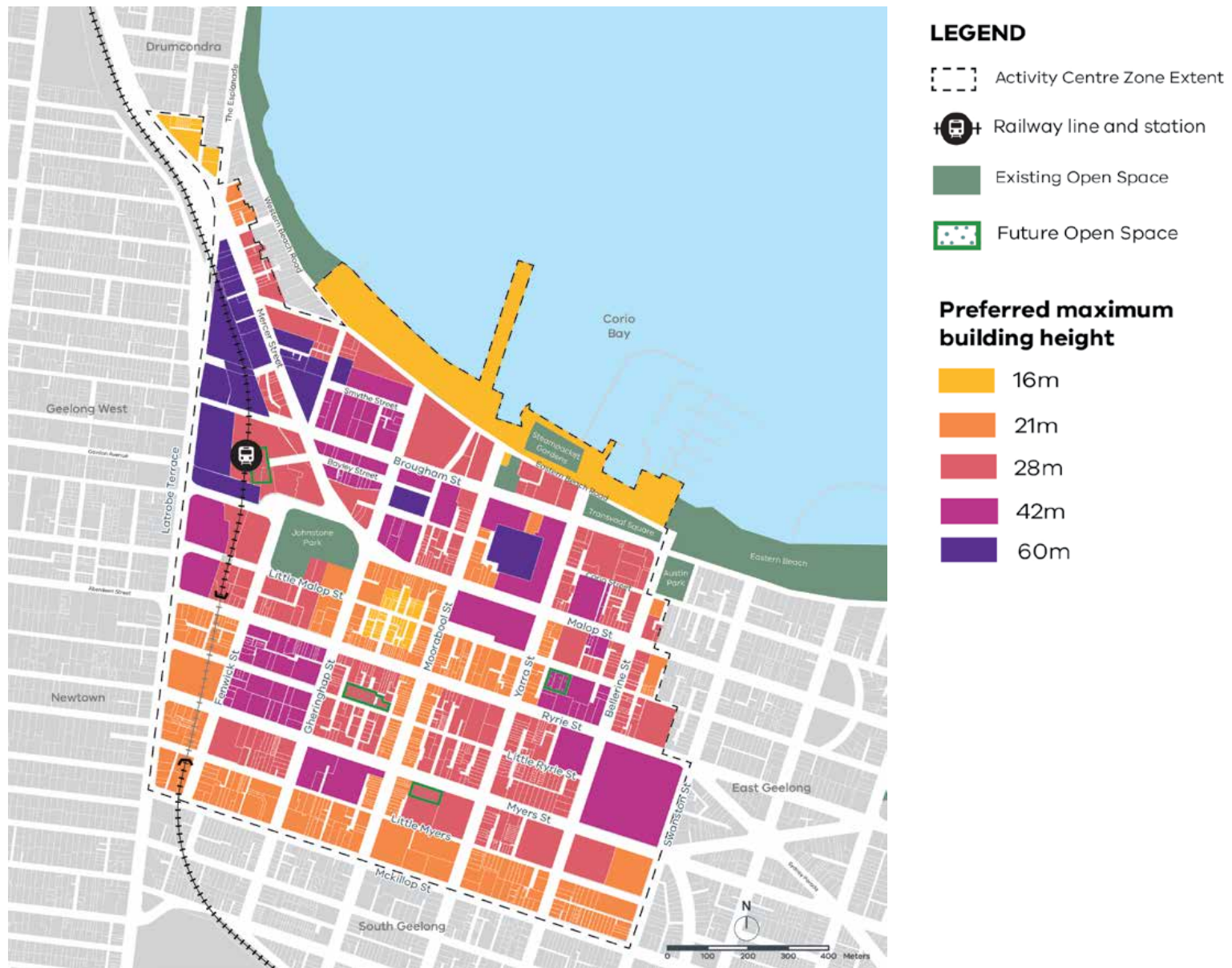
- the alignment of building heights to overshadowing controls
- the alignment of building heights to their precinct’s character and uses and to heritage, grain and character
- development capacity and forecast demand.

### Strategies

- Ensure building scale and form share key views and complement the waterfront and the city skyline.
- Encourage tower elements to be of a slender form and to contribute to the sharing of views of Corio Bay.
- Identify and apply preferred building heights for all parts of the activity centre, derived from each site’s local context, size and characteristics and the preferred precinct character and uses.
- Avoid development that encroaches on sky views and key views of Corio Bay along north-south streets.
- Encourage development facing the waterfront to complement the existing built form, scale and character.
- Avoid the development of isolated buildings above the preferred maximum building height that do not contribute to a varied-yet-undulating city skyline that tapers down to the Geelong waterfront.

Actions	Timeframe	Lead/partner
Action 14. Apply preferred maximum building heights as shown in <i>Figure 8. Preferred maximum building heights</i> as part of an amendment to the ACZ1.	Immediate	DTP

Figure 8. Preferred maximum building heights





## Building height categories

### 16 metres

This height typology is the preferred minimum building height applied within Central Geelong. This category applies to areas that have a fine-grain subdivision pattern, for example, the mainly residential area comprising the York precinct, areas of priority public use such as the Waterfront precinct and areas that are protected by heritage controls such as the small streets and laneways within the Retail Core precinct.

### 21 metres & 28 metres

Both height typologies present a human scale of interaction with their surroundings while supporting complementary development. Development within these heights provides a balanced outcome in areas with high amenity, heritage value and/or sensitive urban character. The 28-metre (eight-storey) typology is applied to areas with less sensitivity to these aspects.

Example areas include mixed-use areas at the edge of the activity centre, small-lot retail frontages and heritage streetscapes (such as those within the Retail Core Precinct), and areas interfacing with the Waterfront Precinct to present a consistent built form that does not create a dominant visual appearance when viewed from public land or Corio Bay.





## 42 metres

This height typology is applied to large allotments favoured for strategic land use and where taller built form elements could be set back sufficiently to reduce their visual bulk while serving as landmarks.

Example areas include land within the Knowledge and Enterprise and Station precincts and generally between Yarra and Bellerine streets, between Little Ryrie and Little Myers streets, between Fenwick and Gheringhap streets and along Mercer and Brougham streets. Proposed built form within this height typology also presents an important anchor point within Central Geelong.



## 60 metres

This height typology is applied where off-site impacts (such as overshadowing) can be avoided or managed and where a new urban character can be established, representing a skyline that reflects Geelong as a regional city.

This height typology has been extensively applied within the Station Precinct and parts of the Knowledge and Enterprise Precinct and selectively applied within the Retail Core Precinct. Buildings within this category should be scaled appropriately when viewed from a distance, and their impacts on their immediate surroundings must also be considered.



## Objective 8. Master plan and coordinate the development of strategic development sites

Central Geelong has several large development sites with the capacity to accommodate substantial commercial and residential floor space. Each site could also be an anchor for its precinct, helping guide its appearance and function.

Given the strategic, unique nature of each site, a master plan should be developed to guide its future development. A master plan provides a vision for the development of an entire site, including how new buildings, access and open space will fit within the existing and planned context. The application of a Development Plan Overlay is the preferred planning scheme tool to establish a master plan requirement and ensure a transparent development process and a cohesively planned Central Geelong.

Several sites (such as University Hospital) have a master plan or are in the process of developing one. An integrated approach by governments and community engagement is essential for strategic development sites owned by local or state governments to unlock their investment potential and ensure development aligns with the vision for Central Geelong.

Density controls in the form of plot ratios have been used in conjunction with built form controls. Applying plot ratios in Central Geelong:

- ensure future development is proportionate to the shape, area and context of its site
- provides a clear, consistent measure to support efficient decision-making and set realistic, clear expectations about the potential development yield on each site
- delivers a range of different building typologies, rather than just developing each site to its maximum allowed height
- gives the architect flexibility to design buildings within the plot ratio and the building envelope from built form controls
- avoids excessive building sizes that could otherwise be achieved on large sites through the use of perimeter-based, built form controls alone
- can support additional benefits (such as new laneways and the retention of heritage buildings and additional open space) by allowing flexible approaches to achieving the plot ratio
- improves the amenity of streets and spaces and helps ensure they are comfortable, attractive, welcoming places.

Plot ratios have not been identified for the Geelong Station and University Hospital sites. Their complexity, unique needs and characteristics as public infrastructure call for detailed planning.

The size of strategic development sites and their significant development potential justifies the provision of open space, including plazas and public-private areas. On some large strategic development sites, there is a need and opportunity to increase inter-block permeability and the amenity of each site and its surrounding area. Therefore, plot ratio controls have been calculated to allow for new through-block links on some larger sites.

Over time, new strategic development sites may emerge (for example, if land holdings are amalgamated). This Framework Plan's approach should be applied to these sites to achieve the best possible outcomes.

## Strategies

- Designate a site a strategic development site and prepare a master plan for it (either as an individual allotment or a combination of allotments) if a site is larger than 4,000 square metres and has at least half of the following criteria:
  - proposes a mix of activities and/or uses
  - will require multiple access points and large servicing areas
  - will require new pedestrian through-links to create greater permeability onsite and across Central Geelong
  - requires, or plans to provide, a mix of private and/or public open space
  - proposes multiple large tower elements above a podium
  - will be constructed in two or more construction phases
  - will share services with adjoining neighbours.
- Introduce plot ratios for strategic development sites within the preferred built form controls in this Framework Plan.
- Deliver new pedestrian links, and encourage the provision of publicly accessible open space on strategic development sites.
- Adopt a whole-of-government approach to facilitating the development of strategic development sites that are publicly owned, in accordance with its precinct’s objectives.
- Engage with landowners of strategic development sites to facilitate investment and renewal consistent with its precinct’s objectives and design quality requirements.

Actions	Timeframe	Lead/partner
Action 15. Apply plot ratios to relevant strategic development sites shown in Central Geelong strategic development sites.	Immediate	DTP
Action 16. Prepare and apply a Development Plan Overlay for each strategic development site as part of an amendment to introduce the ACZ1.	Immediate	DTP



Figure 9. Strategic development sites



## Objective 9. Celebrate, protect and appropriately interpret Aboriginal cultural heritage

For millennia, the Wadawurrung people, as the traditional custodians of lands and waters, have lived in close connection with the landscape in and surrounding Central Geelong.

This connection to Country has continued since the arrival of Europeans in a variety of traditional and contemporary ways. While there are many stories and places in Central Geelong that evidence this ongoing connection, it is often not apparent to people who live, work or visit this place.

Places of contemporary and historical Wadawurrung cultural heritage value enrich people's lives, often providing a deep and inspirational sense of connection to the community and landscape.

The transformation of Central Geelong provides an opportunity to reveal the area's rich Aboriginal cultural heritage and to include and provide space for the ongoing interpretation and sharing of cultural heritage that are integral to the contemporary Wadawurrung living cultural heritage.

There is a clear need for tools and strategies to support the Wadawurrung and non-Aboriginal communities to share knowledge about places of cultural heritage significance and to better understand how we can all work together to respect and protect sensitive sites and strengthen cultural heritage.

Specifically, there is a need for a process to improve the development and more broadly the community's understandings of Wadawurrung values and aspirations for Country within Central Geelong and beyond to explain what it means to nurture Country including understanding stories, traditions and cultural practices, both traditional and contemporary.

Once a clear understanding is achieved, a secondary process of translating the principles, objectives and knowledge into appropriate design terminology and process will offer guidance to support a strong and vibrant Wadawurrung culture referenced in the built environment.

### Strategies

- Encourage buildings and spaces that are 'designed with Country' in mind, meaning that they are designed with natural elements and include an understanding and relationship with the cultural landscape they are in.
- Support the recognition and interpretation of the Aboriginal cultural values and heritage in Central Geelong through ongoing consultation and collaboration with the Wadawurrung through engagement with the development industry and other stakeholders.
- Explore opportunities to reveal the cultural history layers of Central Geelong through appropriate design, planting and curated programs.
- Create opportunities to embed Wadawurrung language, design and names in streets, parks, and public buildings in consultation with the Wadawurrung.

Actions	Timeframe	Lead/partner
Action 17. Prepare guidelines that encourage stakeholders to respond to cultural connections to Country when designing and planning new projects.	Short	Wadawurrung RDV CoGG DTP

## Objective 10. Retain, preserve and protect Central Geelong’s heritage built form fabric and character

Central Geelong is a city with a framework of 19th-century streets and laneways and a magnificent portfolio of heritage buildings, many of which are on the Victorian Heritage Register.

The preferred maximum building heights have been designed to align with the scale of Central Geelong’s existing heritage buildings and streetscapes and ensure they are not eroded by future development while providing scope for renewal that respects these valued attributes. Establishing appropriate street wall heights and setbacks will help reinforce the framework of the street grid and help create a more cohesive, walkable-neighbourhood environment. Establishing mandatory setbacks for the development above heritage buildings will ensure the heritage fabric is protected by preserving the three-dimensional aspects of these buildings, avoiding façadism.

Retention of fine-grain allotments and heritage buildings by having lower preferred maximum heights in key character areas and volumetric retention of heritage will ensure Central Geelong’s urban character is retained while allowing for appropriately scaled growth and development. This is particularly so in the Retail Core Precinct, where preferred minimum building heights of four to six storeys (16 to 21 metres) are applied to respond to the predominate low-rise, fine-grain buildings within the laneways network.

### Strategies

- Ensure new development responds sympathetically and innovatively to heritage places.
- Encourage development in the Retail Core Precinct to retain and enhance the historic building fabric that complements the precinct’s character.
- Encourage the retention, preservation and protection of the heritage built form fabric and character.
- Provide front setbacks that are consistent with existing predominant building alignments to preserve the historic character of streets.
- Ensure that setbacks above the street wall reduce the visual bulk of taller buildings to maintain views and daylight and support the precinct’s preferred character and proposed activities.

Actions	Timeframe	Lead/partner
Action 18. Undertake a review of all heritage planning controls within Central Geelong and progress a planning scheme amendment to implement its conclusions.	Medium	CoGG DTP
Action 19. Implement mandatory setbacks above the street wall for heritage buildings and places and apply preferred discretionary setbacks for all other locations as set out in Table 5. Mandatory and preferred minimum upper setbacks above the street wall height from a street and Figure 17. Conserving the integrity of heritage places if the building is built to the boundary as part of an amendment to ACZ1.	Immediate	DTP



# Getting around with ease

As Greater Geelong's and the region's population grows, so will movement in Central Geelong.

As part of Greater Geelong's 'clever and creative' vision, the region is aiming for 50 per cent of all journeys to work to be made by public transport, walking or cycling by 2047. Currently, 87 per cent of trips into Central Geelong are made by car. Increasing active and public transport leads to healthier residents, less congestion on the region's roads and contributes to positive environmental outcomes such as reduced air pollution.

Central Geelong is a hub for regional public transport services, and much of it is accessible by a 20-minute walk from surrounding residential neighbourhoods. Central Geelong has a well-connected network of cycling and pedestrian paths that connect it with nearby and further-away suburban areas.

The street network and public open space take up about 53% of Central Geelong's area, and a further 13% is taken up by car parking and rail transport<sup>2</sup>. Over the past decade, it has been increasingly recognised that urban streets contribute in many ways to the economic, environmental and social functioning of cities, doing much more than just being infrastructure for vehicles. They are also important public places.

Geelong's streets are a conduit for the movement of people and goods, and Central Geelong's urban activity and identity are closely associated with its street network. However, a paradigm shift is needed,

<sup>2</sup> *Central Geelong Land Use and Market Assessment Report*  
– Update April 2022.

away from managing Central Geelong's streets as a transport network for motor vehicles to prioritising pedestrians' needs and improving how people move around Central Geelong. A smarter, more integrated approach is needed that finds the most efficient way to use available space, provides people with choice and occurs in a timely way that keeps pace with projected growth.

## Issues

- Decision-making is not always informed by the preferred function of each street and how it helps increase economic activity.
- Driving to and parking in Central Geelong is the main way people travel to the city, which increases traffic, lowers urban amenity and occupies space that could be used for people and economic activity.
- Through traffic that does not have Central Geelong as its destination results in congestion and further reduces urban amenity.
- Navigation throughout the city is not intuitive; there is insufficient signage to help people move efficiently, particularly for Geelong Train Station.
- Streets are insufficiently safe and comfortable for pedestrians, with obstacles and numerous vehicle crossovers that are difficult to navigate, particularly for people with mobility issues.
- There is a generally poor relationship between activity in buildings and the streetscape, leading to inactive, quiet streets.

## Opportunities

- Ensure transport planning is informed by and integrated with land use planning.
- Ensure the transport network and services contribute to Central Geelong's liveability.
- Establish a clear street hierarchy to support the strategic management of the street network.
- Provide a public transport network that is responsive to people's needs and services in all areas of Central Geelong.
- Build on Geelong Station's role as a major transport hub and gateway to Central Geelong.
- Increase the proportion of people travelling to Central Geelong on foot, by bicycle and by public transport.
- Ensure footpaths and other pedestrian areas are safe, well lit and easily accessible.
- Redirect through traffic, particularly trucks, to other east-west arterial roads south of Central Geelong. In particular, the Bellarine Link will play a much heavier freight route role, once completed.
- Improve wayfinding and urban design treatments that connect pedestrians to key destinations within Central Geelong.
- Make it more convenient and safer to cross main roads and provide more shortcuts through laneways to improve accessibility.





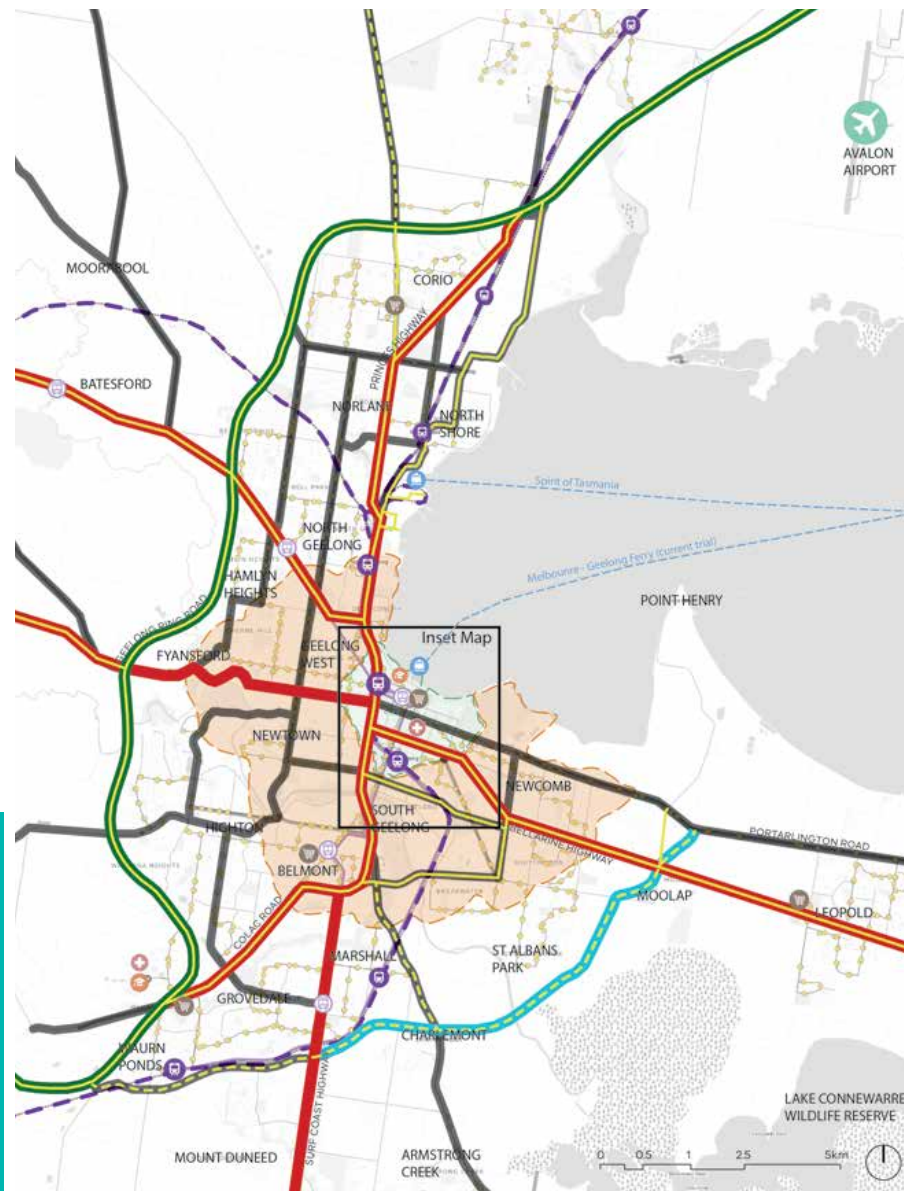
Figure 10. Geelong regional transport context



**Bellarine Link**

The efficient movement of freight throughout the Geelong region is essential to a thriving local and regional economy.

Once complete, the Bellarine Link will play a much heavier freight route role servicing the the Bellarine Peninsula reducing the volume of freight traffic throughout Central Geelong.



**Legend**

**Transport networks**

- Train Station
- Rail Passenger / Freight Line
- Bus Stop
- V/Line Coach Route
- Coach Stop
- Ferry Terminal
- Ferry Route
- Avalon Airport
- 20-minute Central Geelong walking catchment
- 20-minute Central Geelong bicycle riding catchment

**Road & Freight Network**

- Freeway
- Arterial Road (named Highway)
- Arterial Road
- Future Bellarine Link
- Principal Freight Network Road
- Principal Freight Network Road (Planned)

**Regional services**

- Hospital
- Shopping Centre
- Deakin University Campus

## Objective 11. Make Central Geelong a more walkable and safer city

Central Geelong is dominated by cars, freight vehicles and their various needs. This is evidenced by the amount of space taken up by cars within the public environment and the time it takes to cross streets.

Prioritising walking as a mode of transport within Central Geelong will be good for the economy. Many studies have shown that providing a more people-oriented environment in a city leads to more pleasurable experiences, which in turn leads to more people wanting to spend more time in the city. This is particularly relevant for Central Geelong where the experience and amenity it offers is its competitive advantage over Greater Geelong's suburban centres.

*Figure 12. Pedestrian and cycling routes* shows Central Geelong's primary and secondary pedestrian routes, which are intuitive links between precincts, destinations and open space, existing and proposed. Designating and promoting preferred routes and upgrading their amenity, safety and convenience will increase the catchment for walking to and around Central Geelong.

Prioritising walking means addressing barriers to pedestrians' movements, including:

- the need to address the safety concerns of children, women and elderly people, among others
- improving ease of access for people with a disability and other mobility issues
- improving access across the Geelong railway corridor between west and Central Geelong, particularly at Geelong Station
- improving access across major roads (such as Latrobe Terrace and McKillop Street)

- considering reducing speed limits on primary and secondary pedestrian links
- providing shared zones to further support economic activity, pedestrian safety and bicycle access (such as along Little Malop and Corio streets)
- protecting and prioritising pedestrian crossings at intersections and mid-block locations that link key destinations and laneways
- providing new mid-block signalised pedestrian crossings aligned with the proposed future pedestrian links shown in *Figure 12. Pedestrian and cycling routes*.
- providing intuitive wayfinding at key arrival points with links to key attractions and other destinations.

Wayfinding, or the ability to easily find your way to your desired destination, is important to make Central Geelong more walkable. Markers in the street or on the horizon help people navigate, and they can include a combination of streetscape design, signage, activity, landscaping and views to landmarks. A wayfinding strategy needs to understand people's arrival and destination points and identify intuitive or instructive methods to guide their journey along a clear, safe and convenient path.

The *Laneways Action Plan 2018* identified possible new links to improve access to active laneways. These include breakthrough pedestrian links through existing fine-grain lots, which will require engaging with landowners about acquisitions. Also, the redevelopment of strategic development sites provides opportunities for pedestrian links shown in

*Figure 12* due to their strategic location, intensity of activity, diversity of use and overall public benefit.

A first step is progressing a strategic approach, using the Department of Transport and Planning's Movement and Place Framework to develop a long-term, implementable plan. The framework will use the identified street hierarchy (see *Figure 11*) and the pedestrian and cycling route (see *Figure 12*) as the starting point to consult the community about options and solutions to be implemented over time to make Central Geelong a more walkable environment.

Around 30% of freight vehicles on Ryrie Street are not destined for Central Geelong. Removing freight vehicles that do not have a destination in Central Geelong is a priority. Ryrie Street is the site of several major public and private investments, including the Geelong Arts Centre redevelopment and residential development, with public realm capacity to host alfresco dining and other streetscape activations.

The first step to reducing freight traffic on Ryrie Street has occurred through state planning policy which was removed from the Principal Freight Network in December 2021. Also, the Department of Transport and Planning's Geelong Freight Program aims to redirect freight vehicles that do not have a destination in Central Geelong from Ryrie Street to alternative routes, including Fyans Street, Breakwater Road and the future Bellarine Link.

## The Movement and Place Framework

Transport classifications have traditionally focused on levels of motor traffic movement, but the past ten years have seen a revolution in thinking to better reflect the whole spectrum of transport and place outcomes.

The Department of Transport and Planning's new Movement and Place Framework takes a future-focused, multi-modal approach to network planning. It takes into consideration the diverse role places play in planning the types of transport modes appropriate to a local road or street. In this new language, roads and streets are defined by the context of a local place and assigned various 'movement' and 'place' classifications.

The Movement and Place Framework offers progressive ways of working, so a variety of considerations and outcomes in land use and transport planning are better integrated.

## Strategies

- Apply the street network hierarchy shown in *Figure 11. Central Geelong Street hierarchy* for vehicles, pedestrians, public transport and bicycles to clarify objectives for each street in the network and prioritise infrastructure improvements.
- Promote walking as an important mode of transport to get around Central Geelong, particularly within 2 kilometres of Central Geelong.
- Increase pedestrian permeability through blocks, including by encouraging pedestrian through-access at least every 80 metres.
- Improve the surface quality, lighting, footpath width, landscaping and pedestrian preference at the intersections of primary and secondary pedestrian links from surrounding suburbs to Central Geelong.
- Incorporate intuitive wayfinding through the streetscape, signage, activity and landscaping along identified key pedestrian links.
- Provide more protected and priority pedestrian crossings at intersections and mid-block locations that link key destinations and laneways shown in *Figure 12. Pedestrian and cycling routes*.
- Ensure wait times for pedestrians at signals are the minimum time required, including by simplifying traffic signals at intersections along primary pedestrian links.

Actions	Timeframe	Lead/partner
Action 20. Prepare a Central Geelong Movement and Place Plan to prioritise pedestrian and cyclist movement and implement the street hierarchy shown in <i>Figure 11. Central Geelong Street Hierarchy</i> .	Short	DTP CoGG
Action 21. Redirect freight that does not have a destination in Central Geelong from Ryrie Street to alternative east-west routes.	Short	DTP CoGG
Action 22. Implement the <i>Laneways Action Plan 2018</i> , including the priority connection from Denny's Place to Malop Street and from Market Street to Ryrie Street.	Short	DTP CoGG

Figure 11. Central Geelong Street hierarchy



## Objective 12. Provide safe, direct and connected cycling routes

Cycling is an important mode of transport and is encouraged throughout Central Geelong on all streets. Strategic cycling corridors, developed as part of the *Victorian Cycling Strategy 2018–28*, form part of the Principal Bicycle Network and provide the primary network access in Central Geelong and connections to a wider network of routes.

There are currently high-quality bicycle paths along Swanston Street (on top of the kerb, next to the footpath) and sections of Malop, Little Malop and Gheringhap streets (generally on-road and mostly separated from vehicle traffic). Extending routes and connecting them to existing networks will help people ride to Central Geelong, whether it be on short or long journeys.

High-quality infrastructure, including separated bicycle paths, will improve access to Central Geelong from regional trails and connect to high-intensity destinations (such as the retail core, commercial precincts and hospitals). Key streets that could provide for these regional connections and Strategic Cycling Corridors include Gheringhap, Malop, Little Malop and Swanston streets and Western Beach Road.

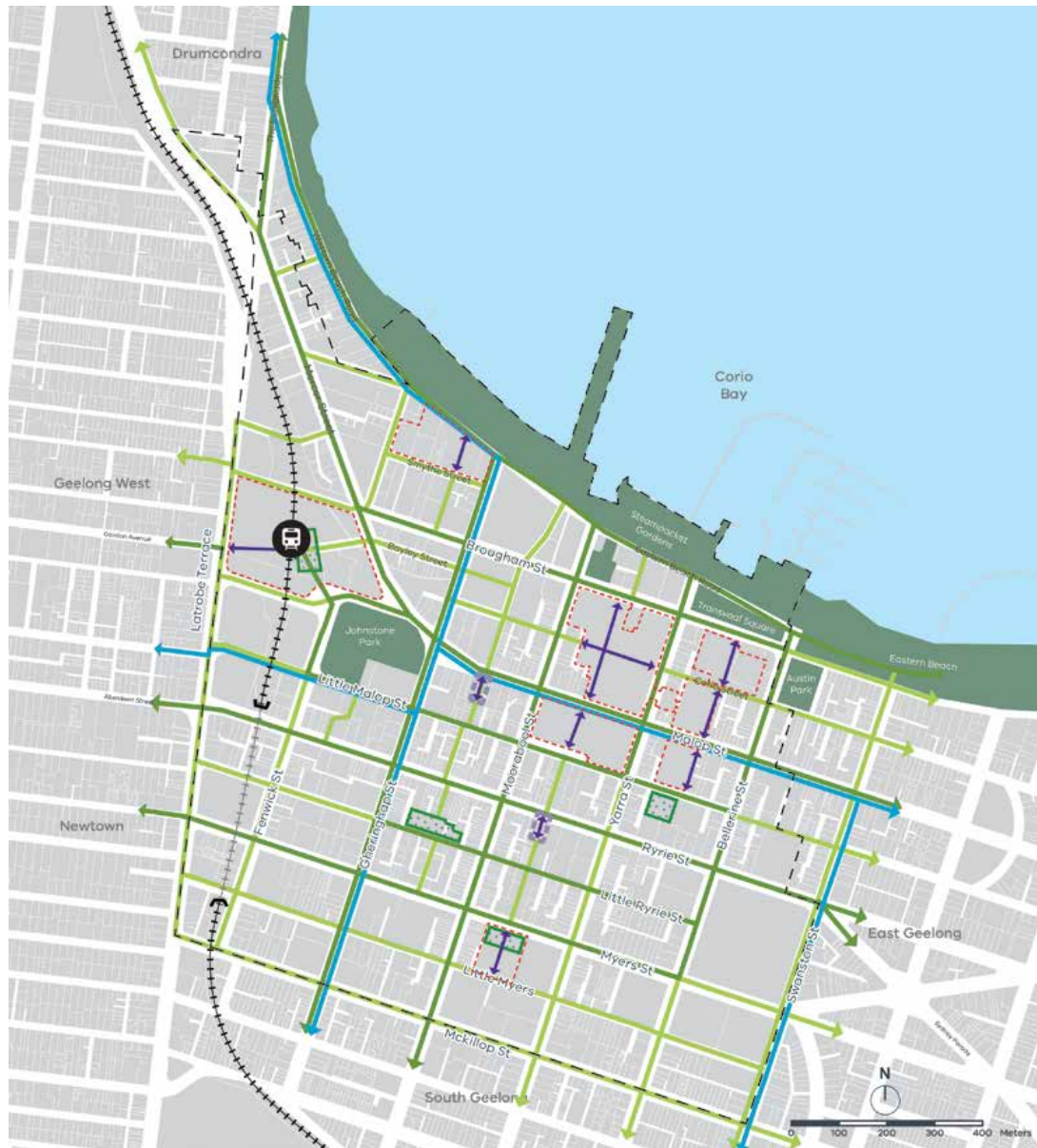
As well as improving bicycle riding infrastructure within the public realm, new development should provide destination infrastructure where possible to further promote and encourage cycling as an important mode of transport.

### Strategies

- Promote cycling as an important mode of transport to get to and around Central Geelong.
- Provide dedicated, separated cycle lanes to streets shown as strategic cycling corridors in Figure 14 and encourage the provision of bike lanes on all other streets and laneways where appropriate.
- Improve the lighting, width and preference at intersections along strategic cycling corridors to and within Central Geelong.
- Prioritise safer pedestrian and bicycle crossings at:
  - Latrobe Terrace and intersections with Little Malop, Myers and Kilgour streets
  - McKillop Street and intersections with Gheringhap, Moorabool, Bellerine and Swanston streets.
- Require new development to provide cycling storage and other related infrastructure consistent with Clause 52.34 of the Greater Geelong Planning Scheme.
- Encourage the provision of separated bike lanes on streets designated as a strategic cycling corridor and on-road for all streets on the Principal Bicycle Network.

Actions	Timeframe	Lead/partner
Action 23. Deliver the strategic cycling corridors shown in Figure 12 and better connect them to Bellarine regional trails.	Medium	CoGG DTP

Figure 12. Pedestrian and cycling routes



**LEGEND**

-  Activity Centre Zone Extent
-  Railway line and station
-  Existing Open Space
-  Future Open Space

**Pedestrian and Cycling Routes**

-  Primary Pedestrian Link
-  Secondary Pedestrian Link
-  Future Primary/Secondary Pedestrian Link
-  Strategic Cycling Corridor
-  Engage with landowners for redevelopment
-  Delivered by Strategic Development Site



## Objective 13. Refine and undertake phased uplifts to Central Geelong's public transport network

Central Geelong's public transport network is currently focused on providing access to Geelong Station rather than providing local access to various parts of Central Geelong.

Planning, refinement and phased uplift are needed to encourage greater use of public transport with better-timed and sufficient bus services that meet the needs of passengers and to cater for projected growth and planned changes in Central Geelong.

The movement routes of buses between Geelong Station and other parts of Central Geelong need to be confirmed to continue to progress the development of the southern section of the Malop Street Green Spine between Gheringhap Street and Malop Street.

Planning to refine and uplift Central Geelong's bus network will need to consider:

- the street hierarchy shown in *Figure 11. Central Geelong Street Hierarchy*, to ensure optimal, efficient traffic management outcomes
- revised alignments with changes to rail timetables as a result of rail duplication between South Geelong Station and Waurn Ponds Station
- planned upgrades to Geelong Station and its broader precinct

- connections with other stations (such as South Geelong and North Geelong stations)
- the need to provide express services for high-demand and longer trips
- more bus priority lanes and signalling
- peak and off-peak travel management
- the location and quality of bus interchange infrastructure to access key destinations.
- The analysis undertaken as part of the *Bus Services Infrastructure Review 2018* of Central Geelong will provide a sound starting point.



## Strategies

- Promote public transport as a preferred mode of transport to get to Central Geelong coupled with a contemporary car parking strategy (see Action 30).
- Ensure that public transport services key destinations within Central Geelong and throughout greater Geelong.
- Improve passengers' amenity at bus stops by providing all-modes accessibility, lighting, information and weather protection.
- Establish Geelong Station's role as a transport hub and interchange point for local and regional services.
- Provide priority to buses along bus routes in Central Geelong, especially during peak times.

Actions	Timeframe	Lead/partner
Action 24. Undertake phased uplifts of the public transport network in Geelong and finalise the ultimate location and configuration of Central Geelong's bus stops.	Medium	DTP CoGG
Action 25. As part of the refinement of the public transport network in Geelong, review opportunities to reconfigure Moorabool Street bus stops.	Short	DTP CoGG
Action 26. As part of phased uplifts of the public transport network in Geelong, refine bus routes to respond to changing travel patterns following upgrades to the Geelong rail corridor.	Medium	DTP CoGG



## Objective 14. Develop the Geelong Station Precinct into a world-class transport hub

Geelong Station is a key public space and piece of infrastructure servicing its community and region. The community depends on it to get to work, access services and undertake social commitments.

The station and its precinct are an important gateway to Central Geelong. At present, the experience of the journey to and from the station is not at a standard many people expect of a station servicing Victoria's second-largest city.

Investments being made as part of the Geelong Fast Rail initiative identify the importance of Geelong as a major gateway. The duplication of the rail line between South Geelong and Waurn Ponds and new services will bring more people through Geelong Station over time.

Master planning of the station precinct is a priority if Geelong is to have a world-class transit hub that connects with its broader urban environment.

### Strategies

- Develop Geelong Station as a major regional transport interchange and landmark area serving as a gateway to Central Geelong.
- Create a strong sense of place and activity in and around Geelong Station and its broader precinct.
- Create a safe, pedestrian-focused public realm in and around Geelong Station that provides easy access to west and Central Geelong.
- Provide year-round solar access for the proposed future station forecourt area.
- Leverage Geelong Station's heritage to build an attractive forecourt that provides a mix of commercial uses at an appropriate scale of development.

Actions	Timeframe	Lead/partner
Action 27. Apply overshadowing planning controls to ensure the future station forecourt area has access to sunlight year round as part of an amendment to ACZ1.	Immediate	DTP
Action 28. Prepare a Geelong Station precinct master plan and implementation strategy that integrates the <i>Geelong Fast Rail</i> and <i>Regional Rail Revival</i> initiatives and delivers on this Framework Plan's objectives.	Short	DTP CoGG
Action 29. Investigate improving pedestrian connectivity between the station and Central Geelong.	Short	DTP CoGG





## Geelong Fast Rail

In the 2020–21 Victorian Budget, the Victorian Government announced \$2 billion towards the first stage of Geelong Fast Rail, matching the Australian Government's \$2 billion funding commitment.

The first stage of Geelong Fast Rail will deliver faster services, with a travel time of about 50 minutes between Geelong and Melbourne. The initiative includes track upgrades as well as upgrades to key stations along the Geelong to Melbourne corridor.

The Department of Transport and Planning is undertaking detailed planning, costing and technical investigations to refine the stage one scope and determine the scope of future investments.

## Objective 15. Strategically manage the provision of car parking

The management of car parking is important for the efficient functioning of the city. While most visitors currently drive to Central Geelong, future growth cannot be accommodated by heavily relying on car access alone. Poorly located and excessive amounts of parking space waste otherwise economically productive space and generate demand for unnecessary trips in private vehicles.

The strategic management of car parking in Central Geelong is a vital part of the overall outcome if Central Geelong is to achieve its full development potential. Any contemporary car parking strategy progressed for Central Geelong should consider:

- the amount of car parking needed (existing and future supply, across various forms) to meet the growth needs of this Framework Plan
- how car parking is managed across Central Geelong which could include for example, charging rates, permit needs, service vehicles and short-term stays

- access requirements which includes technology needs and other information needs to provide awareness, such as through signage and wayfinding
- the planning requirements for car parking in Central Geelong which could include review of statutory minimum requirements for implemented via a Schedule to Clause 45.09 'Car Parking Overlay'.

The appearance of car parking structures is an important consideration in the overall appearance of buildings within Central Geelong. Car parking structures should ideally be located below ground, where this is not possible, car parking elements should be sleeved behind active frontages to a street so as to not be seen from primary street network. Car parks, particular above ground, should also be designed using adaptable reuse methods that encourage commercial floor to ceiling heights to provide options should car dependency reduce with time.

### Strategies

- Ensure parking provisions meet economic growth, traffic management and placemaking objectives.
- Ensure that car parking cannot be seen from the street environment by requiring it to be provided below ground or above grade behind active frontages.
- Maximise economic activity by reducing and discouraging at-grade car parking.
- Allow developments to rely on nearby public parking facilities when seeking to reduce their parking requirement.
- Consider replacing the minimum parking requirement with a maximum requirement and guidance on waivers or cash-in-lieu payments.
- Encourage future all-day commuter parking facilities to be located around the fringe of Central Geelong, outside the Retail Core and Waterfront precincts.
- Encourage activation of ground-level frontages to all streets and laneways in Central Geelong by consolidating the number of driveways and encouraging the innovative use of ground-floor spaces.

Action	Timeframe	Lead/partner
Action 30. Prepare and implement a contemporary car parking strategy for Central Geelong.	Short	CoGG
Action 31. Apply the preferred vehicle cross over and design requirements outlined in the Urban Design Guidelines in this Framework Plan as part of an amendment to the Activity Centre Zone.	Immediate	DTP



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# Vibrant city life

The public realm comprises publicly accessible external urban spaces, including streets, parks, plazas and forecourts. People use these spaces to relax, come together and enjoy events.

The public realm makes a major contribution to the character, comfort, social cohesion and wellbeing of a community. External factors, including buildings and the climate, affect the environment and amenity of the public realm.

Maintaining and improving the public realm for Central Geelong will contribute to achieving the vision. Central Geelong has many high-quality public spaces. Building on this legacy will attract more people to visit, work and live in Central Geelong. Public spaces that are welcoming and green and express Central Geelong's history and design focus will add greatly to its liveability and vibrancy.

There are currently areas within Central Geelong that could provide better amenity, character and environmental outcomes. The public realm needs to be improved to encourage development and businesses, support residential communities, attract more visitors and encourage more people to walk.

## Issues

- Poorly designed buildings that affect the character and amenity of streets and the public realm.
- Limited access to social and community infrastructure to support future population growth.
- Building facades and entrances that present poorly and create pockets of inactivity during the day and at night create safety issues.
- The need for more direct links to improve the safety of pedestrians to the Waterfront and Retail Core precincts.

## Opportunities

- Improve primary and secondary pedestrian links in Central Geelong.
- Promote walking as a key mode of transport to move around in Central Geelong.
- Investigate new pedestrian connections as development occurs, including to the rear of properties on the west side of Mercer Street.
- Protect access to sunlight at ground level, particularly to existing future open space and the Central Geelong street grid.





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## Objective 16. Ensure buildings provide high-quality, interactive interfaces to the street

An attractive and interesting public realm is key to increasing visitation and the length of stay for everyone that comes to Central Geelong. A key principle is the need to add vibrancy and bring to life spaces where people want to hang around and interact socially.

Blank street walls, vehicle crossovers over pedestrian and cyclist pathways, visible above-ground car parking and poorly considered building services detract from the look and feel of a street and reduce the safety of pedestrians and cyclists.

Active frontages enhance and support the economic viability of streets in commercial areas and create a vibrant, pedestrian-focused environment. At street level, clear, unobstructed views into and out of buildings enrich the urban experience for pedestrians and building occupants alike. Transparency in building façades adds visual interest, contributes to a sense of liveliness on the street and improves safety through natural surveillance.

A high degree of visual and physical connection, including multiple entrances and storefront windows, supports active, street-related commercial and retail uses. Attempting to obscure these views creates an uninviting pedestrian realm, and it should be strongly discouraged.

Transparency in façades, especially at the street level, is key to liveability, but it must be balanced with requirements for environmental sustainability, including design measures for energy efficiency, effective lighting, bird-friendly best practice and the use of sustainable materials and construction methods.

### Strategies

- Provide continuity of ground-floor activity along streets and laneways.
- Encourage a mix of activities along streets to attract people after business hours.
- Reduce the number of vehicle crossovers to reduce interaction with cars and create a safe, walking-friendly environment.

Action	Timeframe	Lead/partner
Action 32. Implement active frontage requirements set out in Table 6. Preferred street activation, by land use, <i>Table 7. Preferred glazing requirements for active frontages and Figure 23. Active frontages and glazing requirements</i> as part of an amendment to ACZ1.	Immediate	DTP



Barwon Security

Freckleduck

FD



STANNOOR

## Objective 17. Protect existing and provide new high-quality, accessible open space

For any city, publicly accessible open, green areas are the lifeblood of the urban environment. They promote healthy lifestyles, focus community pride, foster social interactions and help create a sustainable future.

Central Geelong has well-established parks and open spaces that enhance the urban environment and character of neighbourhoods while providing passive and active recreational opportunities. Protecting existing and proposed future open space from overshadowing is essential to ensuring they provide places for people’s wellbeing and enjoyment.

While Central Geelong has ample open space north of Malop Street, there is little to the south. The City of Greater Geelong’s 2020 *Our community places, spaces and services: A social infrastructure plan for the City of Greater Geelong* identifies the need to address open space needs in Central Geelong. Within Central Geelong, the City of Greater Geelong owns several sites on Myers, Little Malop and Little Ryre streets that are currently used as at-grade car parks.

Opportunity exists on large development sites to provide publicly accessible, landscaped, open space. These sites will typically be privately-owned and maintained but should read as public places and be designed to encourage year-round public use. The

location of open space on a site, along with its type, size and intended use, will vary depending on building use, site characteristics and the availability of open space in the surrounding area.

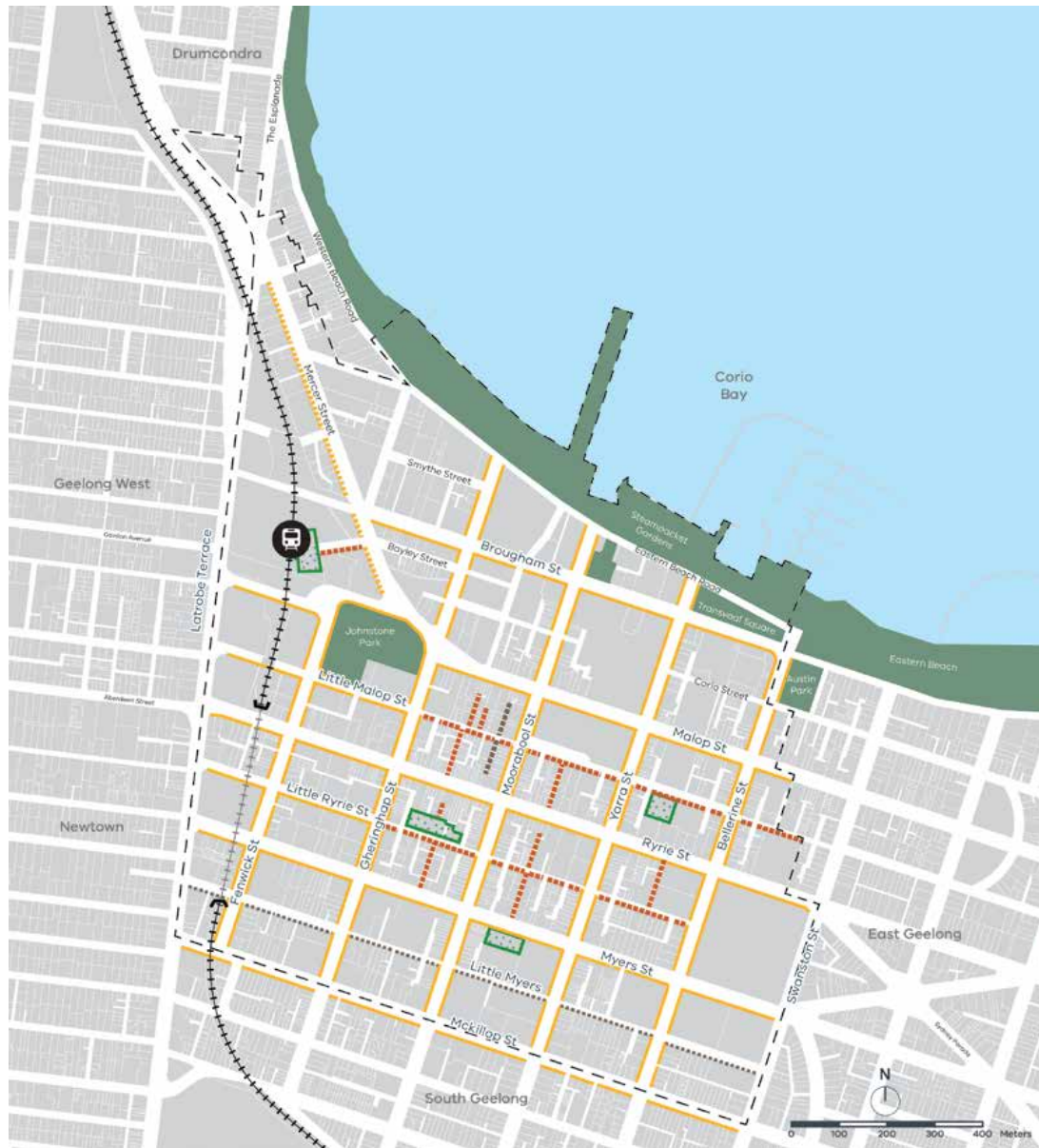
The Geelong foreshore is an integral aspect of the unique lifestyle that Central Geelong offers. A review, refresh and consolidation of the *Central Geelong Waterfront Masterplan 2011* is needed to leverage new opportunities (such as cruise ships), increase economic activity and ensure the foreshore development is integrated. Consolidation of the masterplan’s strategic planning work will reinforce the waterfront as a key destination in Central Geelong.

### Strategies

- Protect sunlight to current and future open space measured at the winter solstice.
- Plan for public open space to be safe, comfortable and accessible to all, supporting people to recreate, congregate and socialise.
- Facilitate the more productive use of council-owned, off-street, at-grade car parks as public space, combined with other potential uses and development.
- Encourage the provision of public space as part of all major development sites, where possible.

Action	Timeframe	Lead/partner
Action 33. Implement mandatory overshadowing requirements shown in <i>Figure 13: Protecting open space and streets from overshadowing</i> and <i>Table 8. Overshadowing requirements for public open spaces</i> found in the Urban Design Framework section of this Framework Plan, as part of an amendment to ACZ1.	Immediate	DTP
Action 34. Commence planning to address Central Geelong’s open space needs, including the future open space shown in <i>Figure 14. Public realm opportunities</i> .	Medium	CoGG DTP
Action 35. Progress an updated Central Geelong Waterfront Masterplan, incorporating the objectives of the Western Beach Masterplan.	Short	CoGG DTP RDV

Figure 13. Protecting open space and streets from overshadowing



**LEGEND**

- Activity Centre Zone Extent
- Railway line and station
- Existing Open Space
- Future Open Space
- Overshadowing of streets and laneways**
- Primary North-South or East West Street
- Secondary North-South Street
- Narrow Street / Laneway A
- Narrow Street / Laneway B

**Protecting sunlight to open space, streets and laneways**

The amenity and quality of Central Geelong’s public realm are key to its continued success as a city centre with a growing population.

Providing year-round overshadowing protection to existing and future proposed open space will ensure these places thrive and continue to be inviting places in which to rest and recreate. As well, primary streets and key laneways need adequate sunlight during the middle of the day at the times of the year when the sun is lower in the sky.

The application of mandatory overshadowing controls within the context of Figure 13 are explained in more detail in the Urban Design Guidelines section of this Framework Plan.

## Objective 18. Provide high-quality streetscape environments

Central Geelong enjoys a high level of pedestrian accessibility, thanks to its 19th-century grid of streets and laneways. Also, the changing role of laneways — from a traditional servicing role to places of activity — means they must become more accessible by completing the missing links in the pedestrian network shown in *Figure 12. Pedestrian and cycling routes*.

Cities around the world are embracing linear parks to support health, amenity, sustainability and wellbeing. The Malop Street Green Spine project is transforming Central Geelong with a vibrant linear park along the length of Malop Street, connecting Johnstone Park and Eastern Park.

Sunlight to primary streets and key laneways is a major contributing factor to delivering high quality public realm environment. Ensuring access to adequate sunlight during the middle of the day at the times of the year when the sun is lower in the sky provides a more inviting, pleasant streetscape.

The Malop Street Green Spine has greatly improved the appearance, function and amenity of the blocks completed so far. Completing this important, city-shaping civic infrastructure must be a priority. To complement Malop Street, Gheringhap Street is to be prioritised as the next north-south green spine. A Gheringhap Street green spine would connect the waterfront, Deakin University and the future Geelong

Convention and Exhibition Centre with Geelong’s civic heart centred around Johnstone Park. Continuing south, it would connect a vibrant commercial and residential area to Kardinia Park.

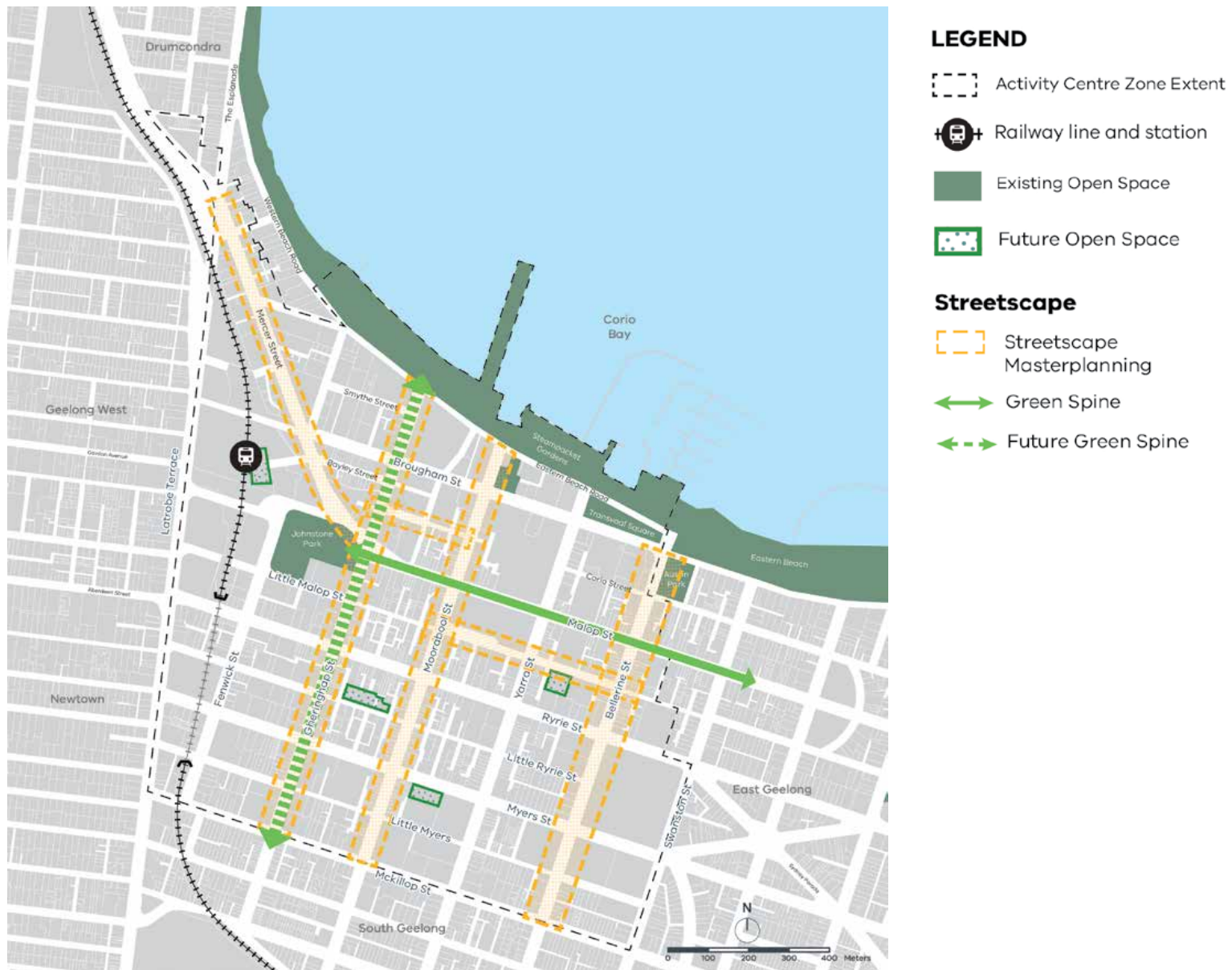
The combination of the above links would provide a more accessible Retail Core Precinct allowing pedestrians to move between locations more quickly, with additional foot traffic helping to support existing businesses on major streets and further support the emerging laneway culture.

### Strategies

- Protect sunlight measured at the September equinox to key streets and laneways.
- Provide weather protection along primary and secondary pedestrian links, pedestrian priority streets and within public open space.
- Facilitate and encourage the flexible use of street space for outdoor events, dining and trading.

Action	Timeframe	Lead/partner
Action 36. Apply overshadowing controls to streets and laneways shown in <i>Table 9. Overshadowing requirements for streets and laneways</i> and <i>Figure 13. Overshadowing of streets and laneways</i> in the Urban Design Framework section as part of an amendment to ACZ1.	Immediate	DTP
Action 37. Complete all stages of the Malop Street Green Spine project.	Short	DTP CoGG
Action 38. Design and deliver the Mercer and Gheringhap Streetscapes Master Plan.	Short	CoGG
Action 39. Progress the streetscape master planning shown in <i>Figure 14. Public realm opportunities</i> .	Ongoing	CoGG

Figure 14. Public realm opportunities



**LEGEND**

Activity Centre Zone Extent

Railway line and station

Existing Open Space

Future Open Space

**Streetscape**

Streetscape Masterplanning

Green Spine

Future Green Spine

## Objective 19. Provide social infrastructure and utilities to meet growing needs

Central Geelong’s projected growth and future development will require the timely delivery of adequate community and utility-based services to meet the needs of a significantly increased workforce and resident population.

Given Central Geelong’s very low resident population, there is insufficient community infrastructure to support the projected future population and workforce. As Central Geelong grows, so will its need to accommodate a more diverse range of residents and particularly to ensure there is adequate child care for families. Central Geelong’s attraction as a lifestyle destination close to the water will also attract older people who are retiring or downsizing.

The *Central Geelong Community Infrastructure Strategy 2017* assessed the community facilities and services Central Geelong needs over the following 10 years. Given the growth scenarios in this Framework Plan, community infrastructure needs should be reviewed and adjusted where necessary to account for the projected growth.

The *Central Geelong Utility Infrastructure Upgrade Strategy 2017* assessed the water, power, telecommunications and other servicing infrastructure Central Geelong needs over the following 10 years. The strategy concluded that Central Geelong’s existing and planned utility infrastructure had sufficient capacity at the time to cater for forecast demand over the 10 years.

### Strategies

- Ensure that community infrastructure is flexible and adaptable to meet the needs of a dynamic, changing community over time.
- Consider the needs of culturally diverse groups for access to places for cultural celebration, support networks and services.
- Deliver universal access requirements as part of all new development for people with disabilities to support social inclusion.
- Provide smart infrastructure (such as public Wi-Fi and charging facilities) in strategic locations to digitally connect people and places.

Action	Timeframe	Lead/partner
Action 40. Review the Central Geelong Community Infrastructure Strategy to align with this Framework Plan’s objectives.	Short	CoGG DTP
Action 41. Implement the Central Geelong Utility Infrastructure Upgrade Strategy.	Ongoing	CoGG

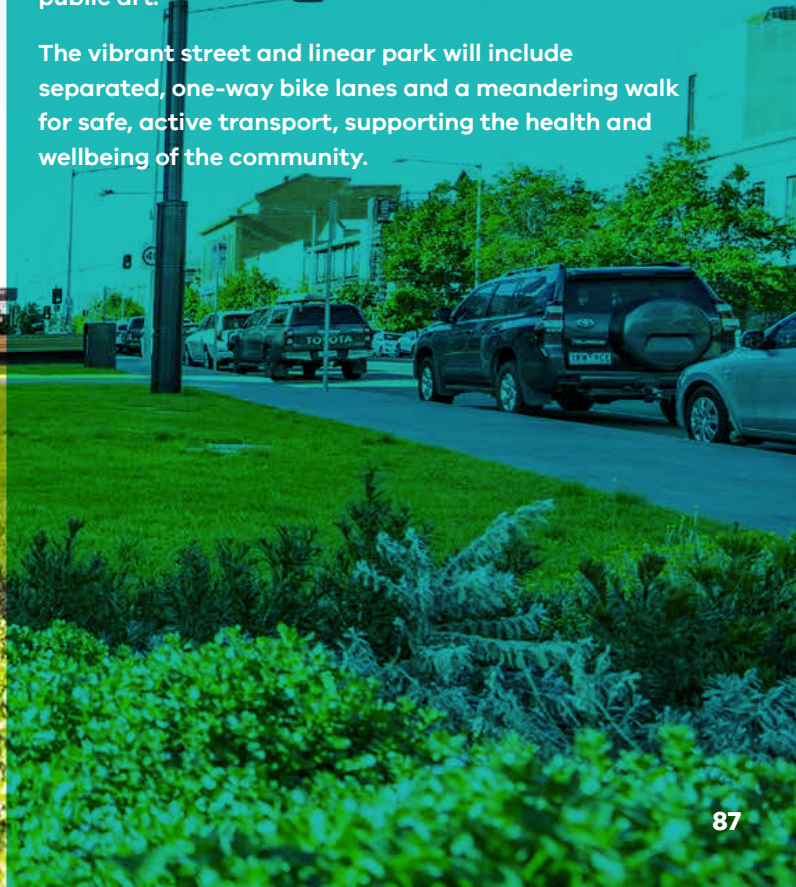


## Malop Street Green Spine

The Malop Street Green Spine will ultimately connect Johnstone Park to Eastern Park and the Botanical Gardens along six blocks of Malop Street.

The green spine is a key project in the revitalisation of Central Geelong, providing nearly a hectare of new green space with stunning indigenous and exotic plantings, as well as seating, alfresco spaces and public art.

The vibrant street and linear park will include separated, one-way bike lanes and a meandering walk for safe, active transport, supporting the health and wellbeing of the community.



## Objective 20. Encourage sustainable development that responds to climate change

Best practice in environmentally sustainable design is essential to Geelong’s status as a UNESCO City of Design.

Climate change will exacerbate the urban heat island effect and raise temperatures at ground levels where most surfaces — concrete and asphalt — are impermeable and have great thermal mass. To adapt to a changing climate, the public realm and built form outcomes must repeatedly and sustainably engage with nature with biophilic design: the practice of connecting people and nature within our built environments and communities.

Central Geelong currently has little tree canopy cover. Increasing the amount of vegetation across the city, within the public realm and incorporated into development will lower temperatures in the hot time of the year and provide far-reaching amenity benefits.

Through planned improvements to streetscapes, Central Geelong will aim to double its tree canopy cover by 2050. Greening will involve species that are appropriate to Geelong’s coastal climate and preferably indigenous to their locality.

Developments will be encouraged to maximise urban greening by introducing vegetation into the design of their buildings at ground level, as part of façades and at rooftop levels. This will further help reduce the urban heat island effect and create better connections with nature.

New buildings will be encouraged to achieve the highest energy-efficiency standards to ensure thermal

comfort and reduce energy consumption and water use. Buildings will be required to be resilient to Geelong’s coastal marine environment and be able to withstand increased corrosion, humidity and moisture to ensure building performance and aesthetic quality are not compromised.

### Strategies

- Design the public realm to provide urban cooling benefits through shading, planting and integrated water and stormwater management for cooling and irrigation effects.
- Require apartment buildings to achieve at least a 6 star Green Star rating and individual apartments at least a 5 star rating.
- Encourage buildings to meet a standard of 40% total surface area as green cover.
- Ensure building designs comply with the principles in the *Siting and Design Guidelines for Structures on Victorian Coast 2020*.

- Ensure materials are appropriate to a site’s location and consider its exposure to coastal marine conditions.
- Where possible, apply the principles of biophilic design to connect buildings and people more closely with nature.
- Ensure buildings are designed to reduce energy consumption through passive solar techniques, adequate thermal efficiency and habitable areas with good access to daylight.
- Minimise environmental impacts and ongoing maintenance by selecting appropriate building materials in the design phase.
- Prioritise building reuse, adaptation and extension if practical rather than demolition and rebuilding.

Action	Timeframe	Lead/partner
Action 42. Investigate the use and application of an appropriate measurement tool to determine a building’s green rating.	Medium	CoGG DTP
Action 43. Apply the built form controls in the Urban Design Framework section as part of an amendment to ACZ1.	Immediate	DTP









# Central Geelong's precincts

**This section outlines development objectives associated with each of Central Geelong's nine precincts and how they will contribute to this Framework Plan.**



# Introduction

This Framework Plan defines nine precincts, shown in *Figure 17. Precincts*. The precincts are based on emerging patterns of use and development centred on seven themes:

- arts and culture
- urban living
- retail and hospitality
- medical, health and wellbeing
- education, office and services
- visitors and tourism.

Central Geelong's precincts provide a foundation for a network of well-connected places containing businesses, institutions and neighbourhoods with opportunities for new employment and expanded health, education, recreation, transport, tourism and housing choices.

Figure 15. Central Geelong precincts



# Bayview Village

**The Bayview Village Precinct is one of Central Geelong’s premier visitor destinations. It provides a vibrant mix of restaurants, bars, hotels and serviced-apartment-style accommodation that interacts with Central Geelong’s premier asset, its waterfront. The precinct will continue to provide for a mix of land uses while delivering outstanding design outcomes in this visually prominent location.**

The Bayview Village Precinct abuts Eastern Beach Road with the Waterfront Precinct to its north and the Retail Core Precinct to its south. Views of this precinct form a dominant aspect of the skyline when viewed from the foreshore and the waters of Corio Bay.

Development within the precinct is of moderate height. It provides a variety of styles of short-stay accommodation in the form of serviced apartments and hotels. Restaurants, bars and other visitor-oriented uses and development front Eastern Beach Road.

A maximum building height of between 21 metres and 28 metres and a maximum street wall height of 21 metres will be preferred for most of the precinct. This will maintain consistency with recent development,

provide for a transition to taller built form towards the centre of Central Geelong and protect the precinct’s maritime heritage.

The area south of Transvaal Square can potentially provide for and extend interactions between the precinct’s visitor- and tourism-related activities and Geelong’s waterfront. The former City of Greater Geelong office site and adjacent car park offer an opportunity to deliver a world-class development, further contributing to this precinct’s role as Geelong’s premier visitor destination. Due to its size, prominence and potential, this site has been identified as a strategic development site.



**Above:** The Bayview Village Precinct provides the first impressions of built form on the skyline of Central Geelong. Building height between 21 and 28 metres provides a consistent and is modest in scale with the Waterfront Precinct.

## Precinct objectives

- Prioritise the use and development of land for visitor-oriented services which contribute to the Geelong economy.
- Ensure new development presents the highest quality in design, particularly areas that face the waterfront.
- Support buildings of not more than 28 metres in height and ensure building scale is in keeping with the established character fronting the waterfront.
- Encourage retail and hospitality generating land uses along identified active frontages.
- Ensure back of house and car parking are located away from active frontages and where possible consolidated to reduce their impact.
- Encourage a coordinated, master planned approach to the redevelopment of the former City of Greater Geelong Office site and related land as a Strategic Development Site.



**Above:** Built form outcomes in this precinct should ensure design excellence which means built form demonstrates how it responds to its natural setting, its sensitivity to heritage buildings, and deliver high levels of architectural merit.



**Above:** The former City of Greater Geelong office site and car park provide opportunity to deliver a world class development further contributing to this area's role as Geelong's premier visitor destination.

## Cultural Precinct

**The Cultural Precinct will continue to be a host for creative arts, library and learning, public gardens and cultural Town Hall events and activities. The civic institutions and spaces will frame Little Malop Street as a central, pedestrian-focused event space and spine connecting the precinct to the Retail Core Precinct.**

The Cultural Precinct will continue to evolve as a vibrant, extended-hours events, community and cultural hub through a continuing program of enhancement and adaptive reuse of important heritage buildings, existing cultural facilities infrastructure, laneways and streets. Its focus will be Little Malop Street.

This precinct will mainly have commercial uses and development. Residential development is unlikely to be a suitable land use and will likely be incompatible from an amenity perspective with preferred uses.

Johnstone Park will continue to be a culturally significant meeting location and a green sanctuary in an urban environment. This will be maintained by prohibiting any overshadowing by development during the middle of Winter measured on the 22 June (the Wintersolstice).

There will be a preferred maximum building height of between 21 metres and 28 metres for most of the precinct. Built form for areas fronting Latrobe Terrace should have a 6 metre landscape setback to provide a distinct visual separation from Latrobe Terrace and to buffer occupants from heavy traffic noise.

A maximum building height of 21 metres and a street wall of 12 metres will be preferred for development fronting Gheringhap Street. This will protect and respect the existing heritage importance of several significant buildings, in particular the Geelong Town Hall, present an appropriate scale along Gheringhap Street and maintain north-south views to Corio Bay.



**Above:** *The Geelong Library and Heritage Centre celebrates the tradition of great libraries while creating an ambitious, future oriented building and design as an organic structure.*



## Precinct objectives

- Prioritise the use of land within the precinct for arts and culture facilities, education, places of assembly, community facilities and other employment generating activities.
- Protect Johnstone Park from the impacts of overshadowing from neighbouring development.
- To support buildings of between 21 and 42 metres that is of an appropriate scale respecting the existing heritage characteristics which dominate the precinct.
- Encourage retail and hospitality generating land uses along identified active street frontages.
- Present a varied built form character comprising of prominent buildings with landscaped setbacks to Latrobe Terrace.
- Respect and where appropriate, integrate interpretative elements into design outcomes to reflect local cultural and indigenous living heritage at Johnstone Park.
- Manage any adverse amenity impacts of the train line and traffic along Latrobe Terrace through appropriate noise and other attenuation measures.
- Encourage a coordinated, master planned approach to the redevelopment of the 245 Latrobe Terrace as a strategic development site.



**Above:** Johnstone Park was an important water source for Wadawurrung people with a creek running through it and out to Corio Bay.



**Above:** Barwon Water's head office was named the best piece of commercial architecture in Australia in 2018 at the National Architecture Awards.

# Health Precinct

**The Health Precinct, anchored by University Hospital and St John of God Private Hospital, supports a range of medical and health-related services within a mixed-use residential environment. Major transformation and the growth of facilities are anticipated in the short to medium terms, focused on interconnected facilities creating a distinctive mixed-use precinct.**

The Health Precinct will continue to expand to serve a growing population. This growth will become more interconnected, with development potentially including clinical schools for universities and research facilities. These will require specialist housing for workers, consulting suites and short-term accommodation. Related service industry and hospitality providers will complement the precinct’s core service delivery drivers.

To support an active, high-amenity pedestrian street environment, new car parking and back-of-house facilities will be consolidated, and where practicable rear laneway networks and secondary streets will be used. The precinct’s elevated topography presents an opportunity to anchor each hospital as a major landmark. Bellerine Street will be the main public entry to University Hospital, and Ryrie Street will be the main entry connecting the clinical schools and research partners.

The preferred maximum building height between Little Malop Street and Little Myers Street and between Bellarine Street and Swanston Street will be 28 metres to avoid impacts on the north-south and east-west Emergency Medical Services helicopter flight path area, shown on the planning scheme map as DDO36, to ensuring 24-hour access to Geelong Hospital’s rooftop helipad.

Views of Corio Bay down Moorabool, Yarra and Bellerine streets will be protected with appropriately scaled and set-back development above the preferred street wall height.

The preferred maximum building height for areas south of Little Myers Street will be 21 metres to provide a tapering of heights to McKillop Street. This responds to the fine-grain allotment character established by the many heritage, protected buildings and its interface with the mostly residential neighbourhood south of McKillop Street.



**Above:** The Haymarket Carpark provide opportunity to provide much needed open space in an area that has none at present. There is opportunity for below ground car parking and open space at ground level is one of many innovative options that could be explored.

## Precinct objectives

- Prioritise the use of land for health, medical and related complementary services, such as education, research and development.
  - Support a variety of accommodation forms within a mixed used environment that includes retail, service and office uses along identified active frontages.
  - Support buildings of between 21 and 42 metres in height that respond to the existing character and lot size of the precinct.
  - Support the ongoing growth and expansion of University Hospital Geelong and the St. John of God Hospital.
  - Ensure that any development adjacent to Saint Mary of the Angels Basilica protects and respects its setting and overall heritage value.
  - Encourage a transitioning down of building height, supported by landscaped setbacks towards Swanston and McKillop Streets.
- Utilise Council owned at grade car park sites on Little Malop Street and Myers Street for future open space to increase available open space within the precinct.
  - Encourage a coordinated, master planned approach to the redevelopment of the Commonwealth Land as a strategic development site should it no longer be used for Commonwealth purposes.
  - Encourage a coordinated, master planned approach to the redevelopment of the Haymarket car park as a strategic development site, including the provision of future open space.
  - Provide a new pedestrian mid-block link to enhance the pedestrian experience, mobility and accessibility of the precinct with other precincts.



**Above:** University Hospital and St John of God Hospital are the main anchors of the health precinct. The area surrounding both provide a range of allied health services providing Geelong and its region with a broad range of medical services.

## Knowledge and Enterprise Precinct

**The Knowledge and Enterprise Precinct will be the business heart of Central Geelong, anchored by national, state and regional headquarters. Deakin University provides opportunities to tap into the creative knowledge economy and encourage conglomerate activities that leverage one another, particularly the future Geelong Convention and Exhibition Centre.**

The precinct has a well-defined, walkable character with easy access to Geelong Station. Much of Central Geelong, with its many services and retail offerings, is within a 20-minute walk.

The precinct's main opportunity is to prioritise the use and development of land for commercial offices and other work spaces to service Central Geelong's emerging knowledge and service economy, which will require extended-hours conference, hotel and hospitality facilities. The future Geelong Convention and Exhibition Centre will be an important visual anchor and visitation landmark in the north-west of Central Geelong.

Maintaining a walkable, high-amenity street environment will be critical to supporting a high-quality environment for those who will occupy the workplaces and who will traverse the area. This will ensure that the precinct can thrive and will drive collaborations, opportunities, ideas, and growth.

Views of Corio Bay down Gheringhap Street will be protected with appropriately scaled and set-back developments above the preferred street wall height.

The preferred maximum building height within this precinct has been set at between 28 metres and 60 metres. There are significant buildings within the precinct, including historic woolstore buildings, that provide a reminder of Geelong's industrial past. These should be protected and their setting respected.



**Above:** The Knowledge and Enterprise Precinct presents significant opportunity to provide Geelong with the commercial floorspace it needs to attract and provide jobs of the future.

## Precinct objectives

- Prioritise the use and development of land as the primary location for office, education and convention centre use and development.
- Ensure commercial development occurs within the podium setting of the building with non-commercial uses located above the podium.
- Support building height of between 42 and 60 metres comprising of slender and well-spaced tower forms providing sunlight access to the public realm and views to the sky between buildings from the street that successfully integrates existing woolstore heritage character within the precinct.
- Ensure that development successfully integrates with the existing woolstore heritage character within the precinct.
- Support complementary ground level hospitality and other commercial uses which contribute to a vibrant and active street environment.
- Support short-term accommodation and other visitor/tourist focused uses facing the Geelong waterfront.
- Encourage appropriately scaled and presented development facing the Geelong waterfront with taller elements sited away from the foreshore.
- Protect key views to Corio Bay down Gheringhap Street and Cavendish Street by setting back development to ensure clear views to the water and the sky.
- Encourage a coordinated, master planned approach to the development of the Geelong Convention and Exhibition Centre as a strategic development site.
- Provide new pedestrian mid-block link to enhance the pedestrian experience, mobility and accessibility of the precinct with other precincts.



**Above:** Deakin University's Waterfront campus. An excellent example of adaptive reuse of Geelong's woolstore heritage.



**Left:** The future site of the Geelong Convention and Exhibition Centre.

## Retail Core Precinct

**The Retail Core Precinct, as Central Geelong's retail heart, will mainly comprise retail, service and hospitality venues in a high-quality environment that prioritises pedestrian amenity and activity. Its traditional, fine-grain shopfronts will be complemented by shopping centres that transform into externally focused, accessible, mixed-use environments. The emerging laneway culture will be embraced and expanded. The precinct's many heritage places will be respected, with their character retained and enhanced.**

Encouraging a more vibrant street life, unique heritage character and diverse activities will continue to improve the precinct's user experience.

The precinct's key anchors are Westfield Shopping Centre and Market Square. Both could be transformed into vibrant, open-to-the-air retail environments offering a broad range of activities and experiences. Their central location provides great accessibility to all of Central Geelong, and their large footprint presents opportunities for a range of uses in addition to retail. Future built form for these sites should be master-planned to support identified plot ratios and built form within a preferred maximum building height of between 42 metres and 60 metres.

Beyond these key anchors, the fine-grained pattern of streets and laneways with hospitality and other service-oriented activity will have built form within a preferred maximum building height of 16 metres to 21 metres. Here, Geelong's emerging laneway culture could potentially provide a mix of uses that support activity throughout the day and night, year-round.

Views of Corio Bay down Gheringhap, Moorabool, Yarra and Bellerine streets will be protected with appropriately scaled and set-back upper podium developments above the preferred street wall height.

There are also opportunities to encourage the better utilisation of shop-top areas, including with shared workplaces, for accommodation, creative and service enterprises. Such utilisation would complement the vision for the precinct while supporting adjoining precincts.

Existing pedestrian-oriented links will be maintained and enhanced and new links encouraged to improve access and mobility within the precinct and connectivity with other precincts.



**Above:** Investment as part of the Laneway Action Plan 2018 has transformed Little Malop Street into a vibrant, active and pedestrian friendly space.

## Precinct objectives

- Prioritise the use and development of land as the primary location for shops, food and drink premises and nightclubs at street level and within the podium level of large buildings.
- Support building height of between 16 and 28 metres where the fine-grained subdivision pattern, and heritage character presents a dominant form.
- Support building height of between 42 and 60 metres on identified Strategic Development Sites (Westfield Geelong, Market Square and Officeworks site) providing slender and well-spaced tower forms providing sunlight access to the public realm and views to the sky between buildings from the street.
- Encourage office, accommodation, and other complementary uses splayed behind active frontages and/or above the podium.
- Maintain an attractive high amenity streetscape environment by providing wide, sunlit footpaths, landscaping, traffic calming, and accessible public transport.
- Support, maintain and enhance the unique character of small format retail and hospitality offerings in little streets and laneways.
- Promote the development of the Westfield Geelong site as an urban marker within Central Geelong.

- Protect key views to Corio Bay when viewed from the public realm down Gheringhap, Moorabool, Yarra and Bellarine Streets by setting back development to ensure clear views to the water and the sky.
- Utilise the Council owned at grade car park site on Little Ryrie Street for future open space to increase available open space within the precinct.
- Protect and maintain the visual prominence of heritage buildings, the prevailing fine-grained pattern of subdivision and heritage shopfronts within the streetscape.

- Encourage a coordinated, master planned approach to the development of Westfield Geelong, Market Square and Officeworks sites as a strategic development sites.
- Provide new pedestrian mid-block links and publicly accessible open space, and encourage laneway breakthroughs to enhance the pedestrian experience, mobility and accessibility of the precinct with other precincts.



**Above:** Market Square now stands on the original town square of Geelong. A new master plan, incorporating the city block containing Market Square and Little Malop Central is needed to establish a shared vision and achieve development potential.



**Above:** Westfield Geelong is a major commercial and retail centre. The site has significant potential to offer mixed-use development within an increased density setting. Reinstating the mid-block pedestrian links will be important to the overall pedestrian permeability of the precinct.

## Station Precinct

**The Station Precinct will be a high-density, mixed-use gateway to the city. The precinct will be transformed into an extended-hours hub for apartment living and short-stay accommodation that provides affordable housing choices. The amenity of the streetscape will be enhanced with an active frontage providing a mix of retail, hospitality and other complementary commercial uses.**

The precinct is a gateway into Central Geelong from Greater Geelong, Melbourne and regional Victoria. Arrival in the city via the Station Precinct will provide a positive, memorable connection to Geelong's events, venues and destinations, including the nearby Deakin University, future Geelong Convention and Exhibition Centre, adjoining world-class civic and waterfront facilities and expanding government services hub.

A welcoming arrivals forecourt court will provide a high-amenity and visually appealing environment, including year-round access to sunlight. Together with improved public transport infrastructure and services, the forecourt and its surrounds will be the foundation piece to revitalise the precinct.

The broader precinct will be transformed into a

modern, transit-oriented, high-quality urban environment with a preferred maximum building height between 28 metres and 60 metres.

An active, comfortable and attractive streetscape environment is essential for the precinct's success. Protecting access to sunlight, ensuring development at ground level is presented at a human scale and allowing for a mix of retail, hospitality and other commercial activities will ground the streetscape environment and maximise amenity for residents and visitors.



*Above: Geelong Station is a gateway into the city and for connections to the regional bus network. There is significant development potential in and around Geelong Station. A master plan and implementation strategy will be progressed integrating the Geelong Fast Rail and Regional Rail Revival initiatives in alignment with the objectives of this Framework Plan.*



## Precinct objectives

- Support the growth and development of Geelong Station as a major regional transport interchange hub and a key entry point into Central Geelong.
  - Support building height of between 28 and 60 metres where development presents a slender form and provides well-spaced tower forms providing sunlight access to the public realm and views to the sky between buildings from the street.
  - Manage any adverse amenity impacts of the train line and traffic along Latrobe Terrace through appropriate noise and other attenuation measures.
  - Provide a future Geelong Station forecourt that is not overshadowed, provides an attractive gateway, and is surrounded by appropriately scaled development that contributes to a positive amenity and urban environment.
  - Support a landscaped setback and an increase in active frontages along Latrobe Terrace.
  - Ensure development which directly abuts land within a residential zone is setback and managed to reduce its impact on neighbouring amenity.
- Enhance the safety, amenity, and vibrancy of the public realm to support increased pedestrian activity along all streets and at the entrances of Geelong Station and improve access across the train line to and from Geelong West.
  - Encourage a coordinated, master planned approach to the development of Geelong Station as a Strategic Development Site.
  - Provide new pedestrian links to enhance the pedestrian experience, mobility and accessibility of the precinct with other precincts and to neighbouring areas beyond Central Geelong.



**Above:** There is significant potential within the Station Precinct for large scale mixed-use development. Enhancing the safety, amenity and vibrancy of Mercer Street and Latrobe Terrace will entice more people to live and work within the precinct.



**Above:** An attractive, safe and high amenity station forecourt is an essential part of the Geelong Station role as a gateway location into Central Geelong. Preserving that amenity while master planning work is progressed is an important part of this Framework Plan.

## Waterfront Precinct

**The Waterfront Precinct is Geelong's premier tourist and visitor destination, and it provides a range of experiences for residents and visitors. The precinct provides the recognisable vistas that Geelong is famous for. Development must be carefully managed and deliver exemplar architecture to ensure the precinct remains an attractive and iconic waterfront location.**

Central Geelong's foreshore is part of a broader foreshore area that defines Geelong and its location on Corio Bay. The precinct's foreshore area is anchored by Cunningham Pier, Steampacket Gardens and the Royal Geelong Yacht Club.

This precinct will continue to be carefully managed as a major visitor and tourism attraction and destination, ensuring its parklands are accessible for all and its public realm is an attractive, safe and experiential space.

Opportunities to strengthen the precinct's role include upgrading the Royal Geelong Yacht Club to provide a broader range of leisure and recreation activities for the Geelong community and the broader region.

It is important to protect and enhance people's experience, safety and the overall amenity of the waterfront, and to ensure it continues to be a vibrant, active space. Opportunities include increasing shelter for people, avoiding overshadowing of parks and pathways and providing active frontages to the public realm.



**Above:** *The Geelong skyline is defined by the interaction of built form and the foreshore. The Geelong Waterfront will remain predominately open space and be complemented with visitor style facilities within a low scale building environment that together will help activate the space and continue to serve the community.*

*Image: Simon Haber*

## Precinct objectives

- Protect and enhance the existing amenity along the waterfront foreshore for pedestrians and cyclists while adding to the vibrancy of the public realm.
- Strengthen the role of the precinct as a public space and a tourist and leisure destination by supporting complementary commercial uses, facilities, activities and infrastructure.
- Support buildings of not more than 16 metres providing exemplar architectural design that is in keeping with existing building scale on the foreshore.
- Support, link and leverage off visitor focused facilities and infrastructure, including Corio Bay, open space for leisure and events, Cunningham Pier, and the Royal Geelong Yacht Club.
- Carefully site and design new buildings and works to present exemplar architectural outcomes appropriate for a coastal setting and its visual interaction with the vista of Central Geelong.
- Encourage active frontages of new and existing built form where they front the public realm.



**Above:** Maintaining and enhancing the Waterfront Precinct's role as a public space and major visitor attractor remains an important strategy. Providing visitors with information that helps them explore more areas of Central Geelong from the Waterfront Precinct will help spread the economic return to a wider area.



**Above:** Geelong has a rich maritime history that can be seen across the Waterfront Precinct. Continuing to protect and preserve this rich history and integrating respectful forms of development within this setting is key to maintaining its character and identity.

# West Village Precinct

**The West Village Precinct will be anchored by a distinctive range of uses, including Matthew Flinders College and secondary retail, office and residential uses. The precinct will prioritise a growing residential community with access to jobs and services and convenient access to Geelong Station and the public transport network.**

The West Village Precinct anchors Central Geelong at its south-west corner, providing a mix of uses. The precinct has great potential as a growing residential community with jobs and services within its boundary as well as access to those within the neighbouring Retail Core, Cultural and Health precincts. This precinct is uniquely positioned to offer a diverse range of residential living options, including collaborative housing, affordable housing, build-to-rent, downsizing and home-office styles of residential accommodation.

The preferred maximum building height is between 21 metres and 42 metres. The precinct contains a mix of fine-grained subdivisions, larger allotments and heritage buildings and places. Street wall heights will need to be carefully managed to avoid an imposing street frontage and overshadowing of key streets and laneways. Views of Corio Bay down Gheringhap Street will be protected, with appropriately scaled and set-back upper podium developments above the preferred street wall height.

Improving the interaction of development with landscaped setbacks for areas abutting Latrobe and McKillop streets will help create buffers and less visually intense interfaces with these key transport routes.

## Precinct objectives

- Provide a variety of residential and small-scale commercial mixed use development outcomes.
- Provide an attractive gateway to Central Geelong from the south west through high quality urban design and delivery of appropriate landscaped setbacks along Latrobe Terrace, McKillop Street, Little Myers Street, and part of Fenwick Street.
- Support buildings of not more than 21 metres in height that complements the existing low rise, dispersed built form character of the precinct.
- Maintain and protect the prominence of heritage buildings within an evolving character that provides a high amenity for residents, with building heights transitioning from 21 metres at the periphery of Central Geelong increasing to 42 metres towards its centre.
- Manage any adverse amenity impacts of the train line and traffic along Latrobe Terrace through appropriate noise and other attenuation measures.
- Protect key views to Corio Bay when viewed from the public realm down Gheringhap, by setting back development to ensure clear views to the water and the sky.

# York Precinct

**The York Precinct will offer inner-city living on the city edge near the transformed Station Precinct. Commercial uses abutting Latrobe Terrace and Keera Street will provide an inviting entrance into Central Geelong from the north.**

The York Precinct is a small but important pocket of Central Geelong and the first and main entry point into Central Geelong for those travelling south on Latrobe Terrace. It is neighboured by standard residential development to its north and east.

Land within this precinct can provide sensitively scaled residential development yield within a preferred maximum building height of 16 metres. The precinct is conveniently located, within a short walk to Geelong Railway Station and future commercial areas planned for Mercer Street.

New, preferably commercial development fronting Keera Street/Latrobe Terrace will present an inviting entry into Central Geelong. Development abutting existing residential neighbourhoods to the north and east should be managed carefully to avoid amenity impacts on these areas.

## Precinct objectives

- Provide a variety of residential and small-scale commercial mixed use development outcomes.
- Provide an attractive gateway to Central Geelong from the south west through high quality urban design and delivery of appropriate landscaped setbacks along Latrobe Terrace, McKillop Street, Little Myers Street, and part of Fenwick Street.
- Support buildings of not more than 16 metres in height that complements the existing low rise, dispersed built form character of the precinct.
- Maintain and protect the prominence of heritage buildings within an evolving character that provides a high amenity for residents, with building heights transitioning from 21 metres at the periphery of Central Geelong increasing to 42 metres towards its centre.
- Manage any adverse amenity impacts of the train line and traffic along Latrobe Terrace through appropriate noise and other attenuation measures.
- Protect key views to Corio Bay when viewed from the public realm down Gheringhap, by setting back development to ensure clear views to the water and the sky.





# The Urban Design Guidelines

**This section contains built form guidelines to realise the character, amenity, movement and public realm outcomes envisaged by the Structure Plan.**

This section is presented based on the following themes:

- 1) Site Context
- 2) Site Organisation
- 3) Building Design
- 4) Pedestrian Realm

# Site context

## Protecting key views and vistas

Ensure large development contributes positively to Central Geelong's skyline and protects internal views of Corio Bay and beyond.

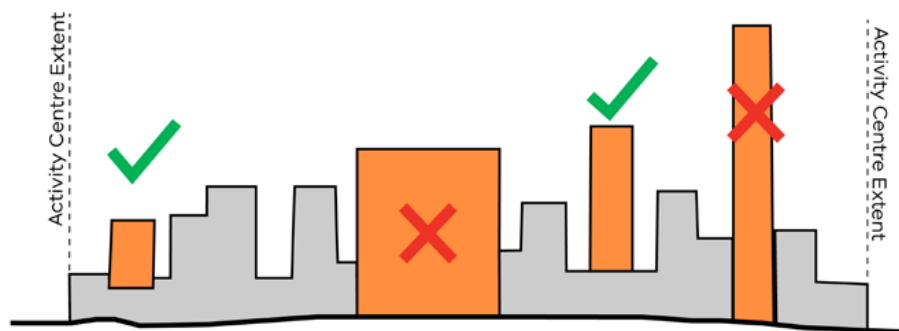
1. Apply a transition in height from the foreshore into Central Geelong consistent with *Figure 8. Preferred maximum building heights*.
2. Provide appropriate setbacks above the preferred maximum street wall height to maintain open views of Corio Bay along north-south streets and key views to key heritage buildings and landmark sites.
3. Position and orient tower elements to enable a sharing of views to Corio Bay.

### Rationale

Central Geelong is bounded by Corio Bay to the north and the Barwon River to the south, with its topography forming an amphitheatre facing the bay.

Central Geelong's traditional landmarks are its church spires, and any buildings above seven storeys are visible landmarks on the skyline. It is therefore important to consider where taller built form can be located, marking key destinations for development (such as the area around Geelong Station).

Views along north-south streets to the bay remain integral to the Central Geelong experience, with the Westfield skybridge providing a poor example: it interrupts key views along Yarra Street. The views from the bay and The Esplanade of the Central Geelong skyline are among Geelong's defining vistas.



**Above:** Avoid isolated tall or bulky buildings that do not contribute to a varied, yet undulating view of the city skyline

**Right:** Providing space and orienting tower elements will provide shared views to Corio Bay and provide sunlight to penetrate between buildings.

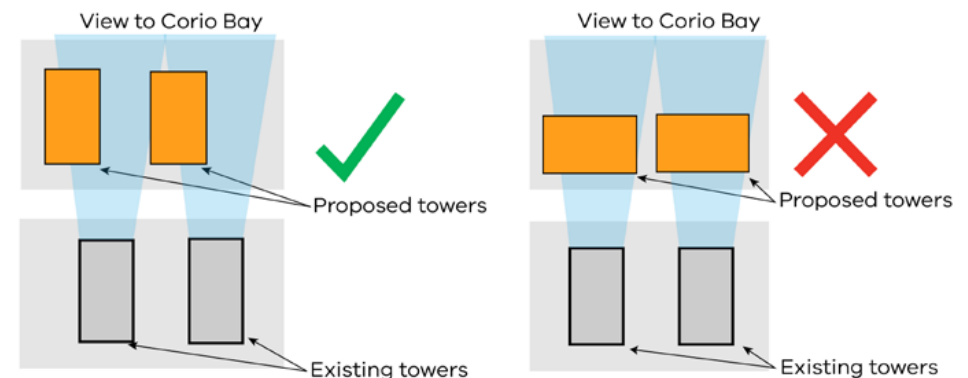
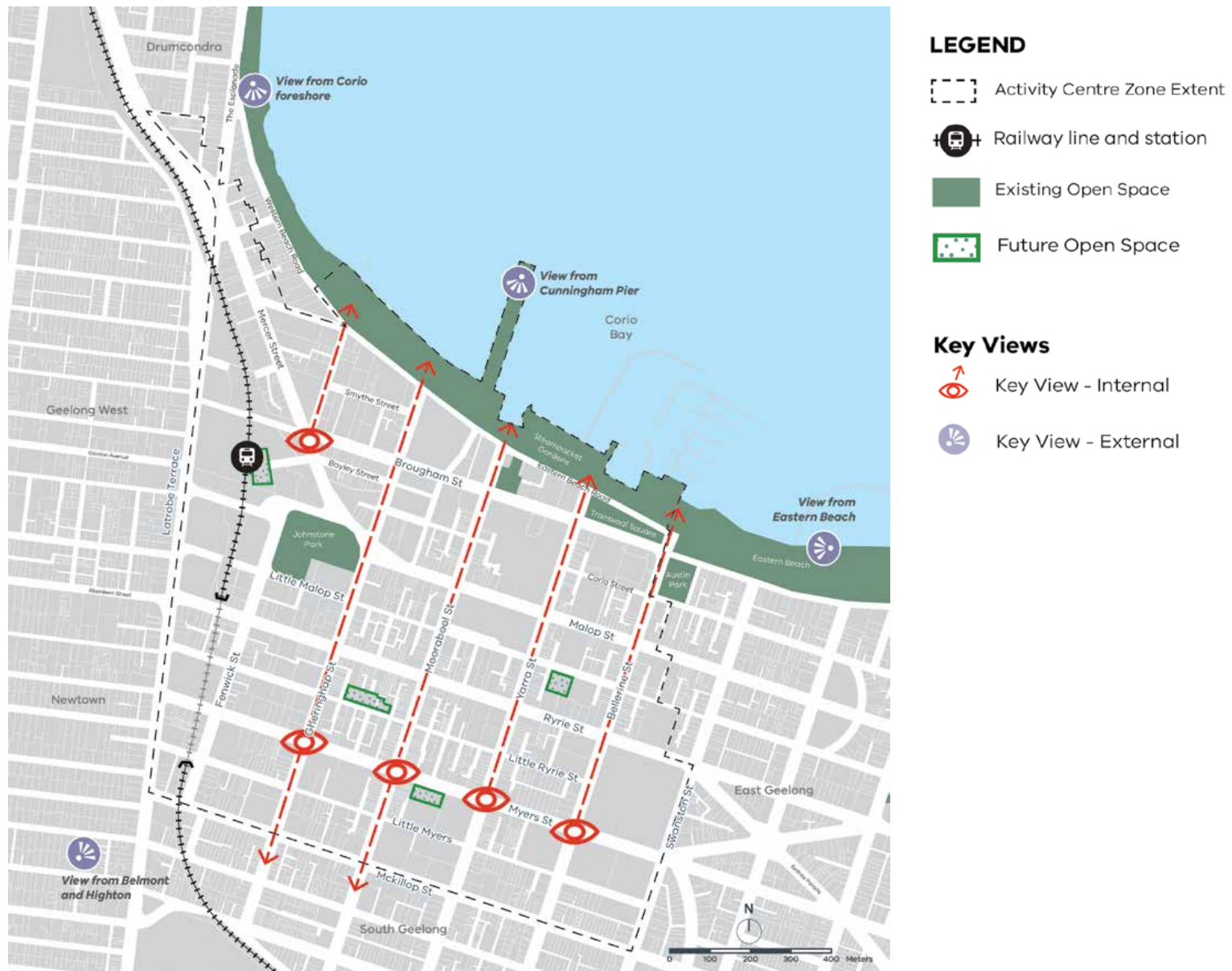




Figure 16. Key views and vistas





*Above: View looking north on Gheringhap Street from Myers Street*



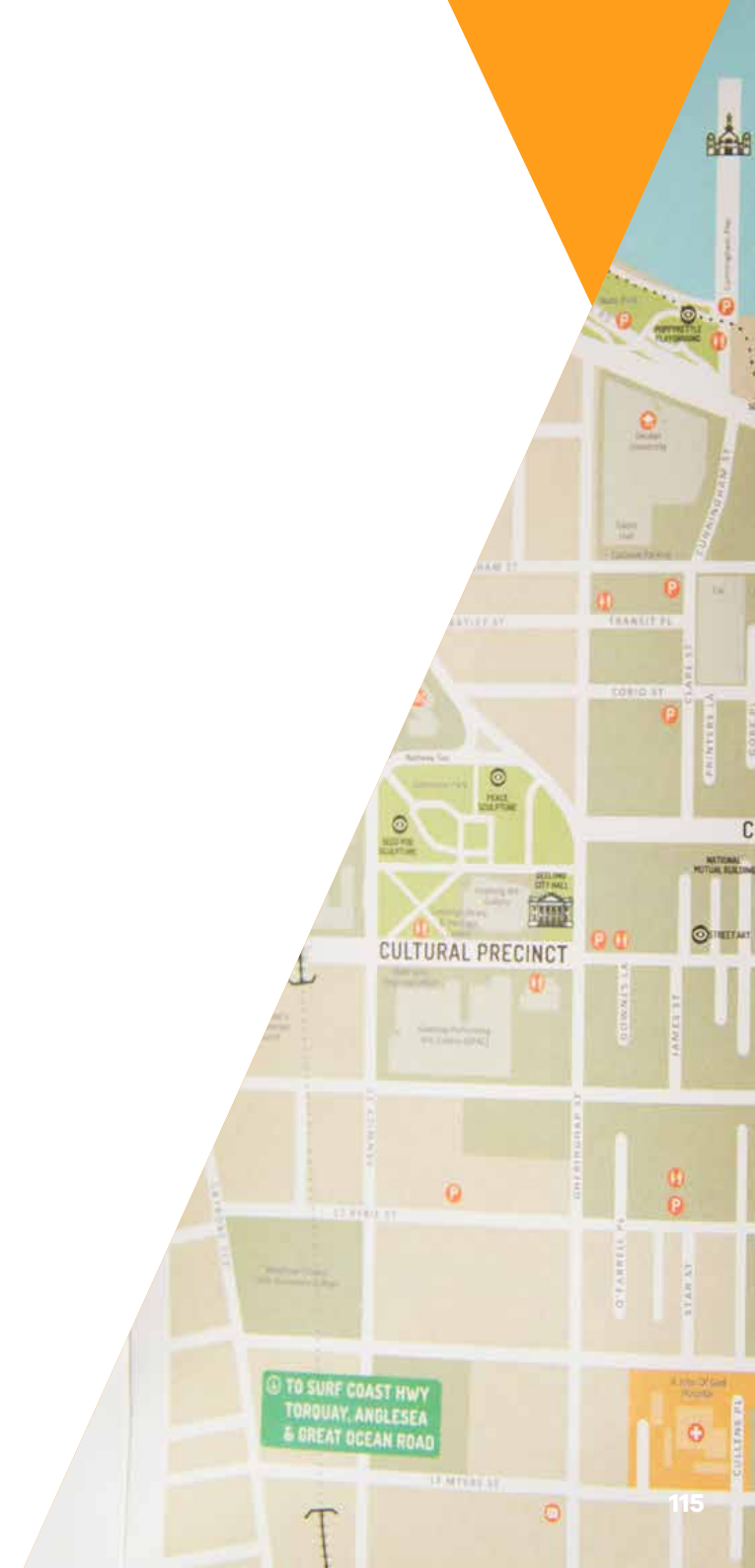
*Above: View looking north on Moorabool Street from Myers Street*



**Above:** View looking north on Yarra Street from Myers Street



**Above:** View looking north on Bellerine Street from Myers Street



# Site context

## Protecting heritage buildings and places

*Ensure development respects and complements the scale, character, form and setting of onsite and adjacent heritage buildings and places.*

1. Conserve the integrity of the cultural heritage values, attributes, character and three-dimensional form of an onsite heritage building or place.
2. When development is proposed adjacent to a lower-scale heritage building or place:
  - a. design podium elements to respect the urban grain, scale, setbacks, proportions, visual relationships, topography and materials of the historical context
  - b. Design the podium to give prominence to the heritage character and an integrated outcome through a respectful use of materials and form.
  - c. provide additional setbacks consistent with *Figure 17. Conserving the integrity of heritage places* to respect the heritage setting.
3. Preserve the three-dimensional heritage built form, including by retaining at least the first 6 metres of the heritage building to its primary street frontage and retaining at least the façade on secondary and other street frontage/s.
4. Preserve the visual prominence of a heritage building located on a corner or where it forms part of a consistent commercial, residential or industrial heritage streetscape.

### Rationale

Central Geelong has a well-defined framework of 19th-century streets and laneways and a magnificent portfolio of heritage buildings, many of which are on the Victorian Heritage Register.

The Geelong community values its heritage buildings and places and expects them to be protected and for development to conserve the integrity of their heritage value, attributes and character, consistent with accepted principles of good heritage conservation.

Some properties are unsuitable for development because of constraints imposed by their size or by their development being incompatible with conserving a heritage building or place.

Where opportunity exists, heritage buildings and places should be used to inform the scale and contextual treatment of the development. If well-designed and sited in appropriate locations, large buildings can make a positive contribution within a historical setting.

There will also be heritage properties that can work in harmony with development. In these cases, development should strive for the long-term protection, integration and reuse of heritage properties.

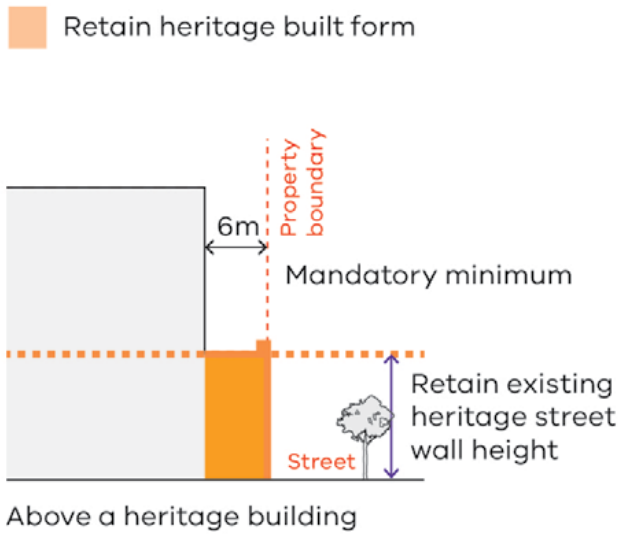


Figure 17. Conserving the integrity of heritage places



**Above:** Heritage buildings and places should be used to inform the scale and contextual treatment of a development.  
Image: MGS Architects

# Site organisation

## Building address and entrances

*Ensure primary building entrances front onto public streets and are well-defined, clearly visible and universally accessible from adjacent public footpaths.*

1. Use high-quality architectural and landscape design to emphasise primary entrances.
2. Differentiate between residential and commercial entrances in mixed-use buildings.
3. Provide to each ground-floor retail unit an entrance that is identifiable and directly accessible from the public footpath.
4. If a site can accommodate more than one tower, ensure that the entrance to each is clearly identifiable, visible and universally accessible from the public footpath.
5. When a larger tenancy is planned, divide the façade into narrower widths or bays and provide multiple secondary entrances to animate the street.
6. Ensure that high levels of glazing is used to create an active frontage between the pedestrian realm and internal commercial uses, particularly on prominent corners.

### Rationale

Well-designed entrances create an arrival experience and identity for the tall building, and they can help define the transition between the public and private realms. Typically, the most vibrant and interesting streets are lined with active, street-related uses accessed by a series of entrances from the public footpath.

Clear, visible entries and views from building interiors to the street provide security for building occupants and pedestrians. Direct, universal access from the public footpath to each tower or use within a tall building animates the street and encourages pedestrian activity to occur in the public realm rather than inside the building.



**Above:** Example artists impression of development on prominent corners providing clear and distinct architectural expression via entrances, windows, canopies, steps, and recesses and projections, should highlight individual elements and reinforce a variety of scales and textures within the podium.

Image: Green Architects



**Above:** Example artists impression of mixed used development on a prominent corner. Prominent corner sites should provide clear and visible active commercial identity. Residential entrances should be separated from commercial entrances and provided further along either street frontage.

Image: MV+A Architects

# Site organisation

## Accessible open space

*Provide a range of publicly accessible open spaces to complement, connect and extend Central Geelong's network of public streets, parks and open spaces*

1. Locate and design publicly accessible open space to:
  - a. read as a public place and include features that encourage year-round use
  - b. provide direct visual and physical connections to public streets, parks and open space, including to and from adjacent pedestrian and cycling routes
  - c. complement and connect with publicly accessible open space on neighbouring properties, where possible
  - d. maximise safety, comfort and amenity, including access to sunlight and clear views to and from adjacent streets and buildings.
  - e. provide universal accessibility, pedestrian-scale lighting, four seasons landscaping, seating, public art and protection from wind and inclement weather.
2. On larger sites, use publicly accessible open space to provide through-link pedestrian connections consistent with *Figure 12. Pedestrian and cycling routes*.

### Rationale

Large-scale developments have important onsite opportunities to provide publicly accessible, landscaped open space. Although such open space is typically privately-owned and maintained, it should read as a public place and be designed to encourage public use.

The location of open space on a site, along with its type, size and intended use, may vary depending on the building's use, site characteristics and the extent of open space available in the surrounding area. Providing good-quality, publicly accessible open space within a tall building site can help development fit within the existing context. It is particularly important when there is little area of public park in the surrounding area.

Publicly accessible open space should be large enough and flexible in its design to support a variety of uses and programming opportunities. The design should also create a microclimate that supports pedestrians' comfort and biodiversity. It should also meet or exceed public standards for universal accessibility, safety and high-quality architectural, landscape and sustainable design.



## Types of publicly accessible open space

**Courtyard:** landscaped open space located in the centre of a single or consolidated block with no direct street frontage.

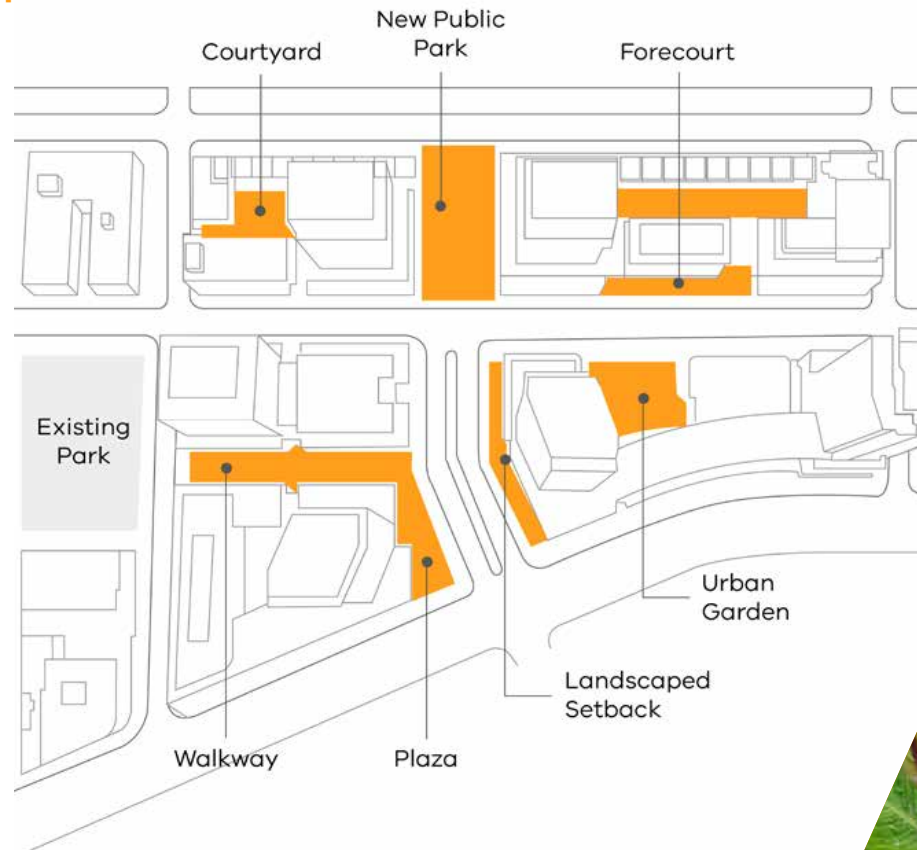
**Forecourt:** landscaped open space between the public footpath and the main entrance of a building.

**Landscaped setback:** a space between the public footpath and the building face, characterised by a hard or soft landscape treatment.

**Plaza:** an animated gathering place with mainly hard-surfaced landscape features flanking a public street.

**Urban garden:** a landscaped space, usually of intimate scale, open to a public street and located and oriented to provide maximum sunlight at midday.

**Walkway:** an exterior public pedestrian route at street level, usually providing a connection through a block. When glazed and enclosed, it is a galleria.



**Above:** Open space can be provided in a variety of forms and places. Providing flexible spaces that are designed to incorporate a variety of uses will help create inviting spaces for people to enjoy the outdoors, , meet others congregate and spend more time in Central Geelong.

# Site organisation

## Vehicle access

*Provide a safe environment for pedestrians to navigate the streetscape environment*

1. Vehicle access should be provided in line with *Figure 18. Central Geelong vehicle access requirements*.
2. New crossovers should not be wider than 6.5 metres.
3. Vehicle access to car parking should be located away from streets located on the primary and secondary pedestrian network noted on Figure 12 to ensure the amenity and safety of these streets for pedestrians.

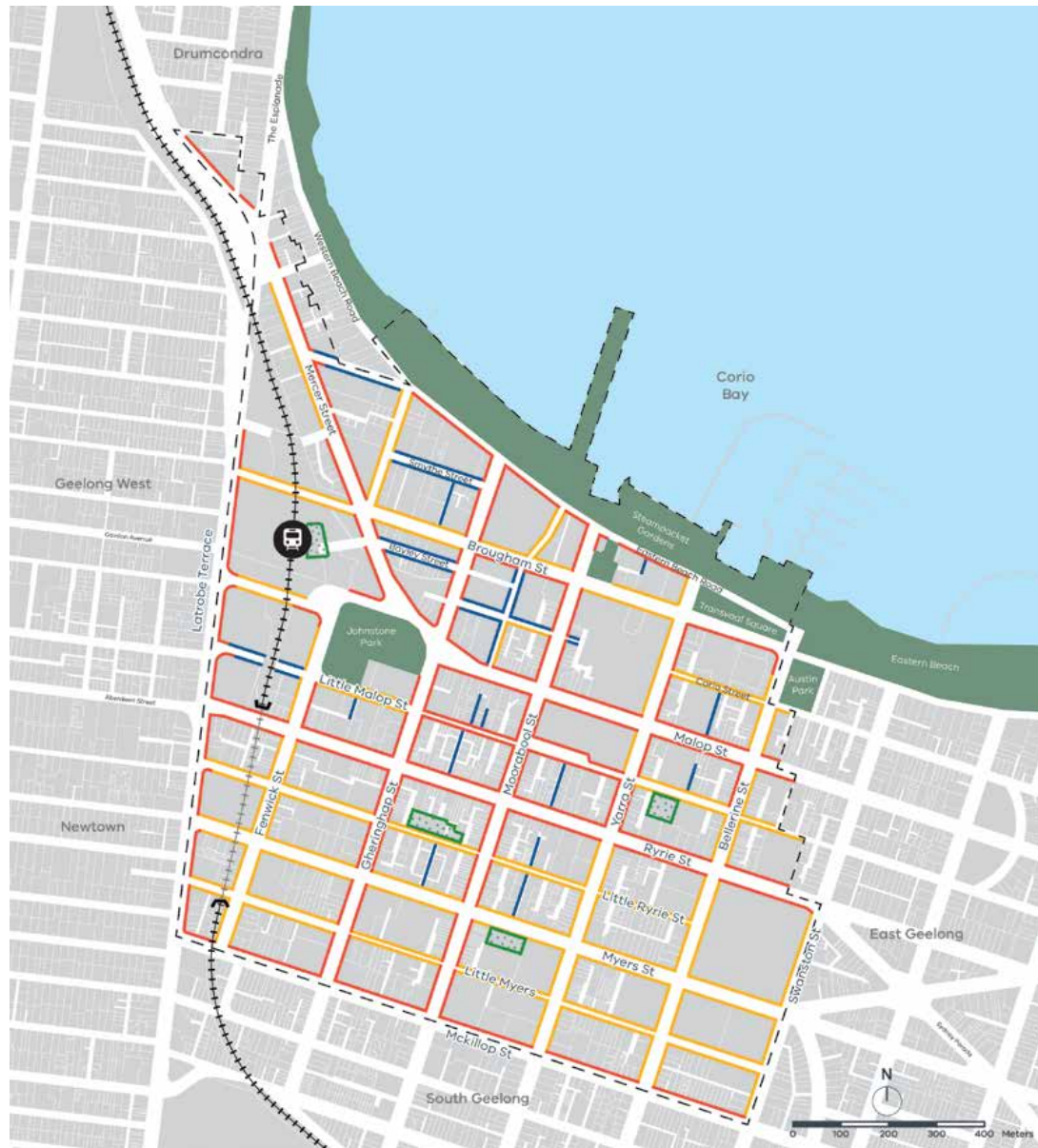
### Rationale

A safe pedestrian environment avoids opportunities for vehicles and pedestrians to interact. Interaction occurs where vehicle crossovers are located. Having fewer vehicle crossovers also contributes to a vibrant street environment that is inviting and easy to navigate.

To improve interactions, designers should avoid locating vehicle access on major streets that also form part of the primary and secondary pedestrian network (shown in *Figure 12. Central Geelong pedestrian and cycling routes*) to ensure the amenity and safety of these streets for pedestrians. *Figure 18. Central Vehicle access requirements* shows these as Category 1 and 2 streets.

Where possible, new crossovers and service access should be provided by an adjoining service laneway. Crossovers on minor streets and active laneways (Category 3 streets in *Figure 18*) should be minimised and consolidated where possible.




Figure 18. Vehicle access requirements



## LEGEND

-  Activity Centre Zone Extent
-  Railway line and station
-  Existing Open Space
-  Future Open Space

## Vehicle Access Requirements

-  Category 1 - No new crossovers. Relocate and remove existing crossovers where possible\*
-  Category 2 - Avoid new crossovers where possible unless no other access point is possible.
-  Category 3 - Minimise and consolidate unless no other access point is possible.

\* Crossovers for emergency access or vehicles are exempted.

# Site organisation

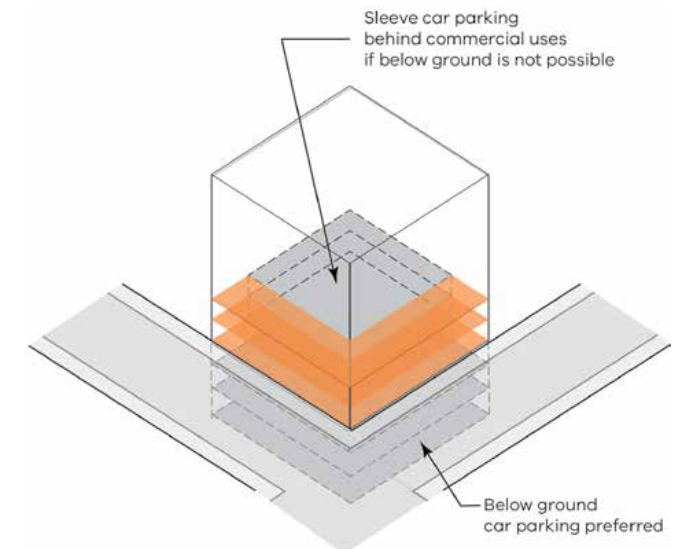
## Car parking

*Encourage car parking structures that are visually subservient when viewed from the street.*

1. Prioritise below-grade car parking to conceal it from public view and make the best use of the outlook by locating habitable/occupied space on the perimeter of the building.
2. Where it is not feasible to locate car parking underground, locate it at-grade or within the upper levels of the building or structure, where it should be:
  - a. sleeved or wrapped by residential, retail, office or other active uses to the street frontage(s)
  - b. detailed to provide visual interest to the laneway, if access is provided via a secondary laneway
  - c. designed to protect sensitive adjacent uses from vehicle noise, vibrations and emissions.
3. Above-ground car parking structures should be designed with level floors and a floor-to-floor height of at least 3.5 metres to allow for adaptation to different uses over time, including to be habitable space.
4. Car parking structures should be consolidated on large development sites to minimise the number of vehicle access points.

### Rationale

Car parking structures should be designed to be easily accessible while being visually subservient within the street. If car parking is not located in basements, then car parking structures should be sleeved with active uses that maximise opportunities for passive surveillance. These structures should be designed to allow for adaptation for commercial or residential uses as car dependency reduces over time or higher-value uses are prioritised.



**Above:** Car parking of any building should not form a major part of what is viewable from streets within Central Geelong. Entrances should be consistent with those shown in Figure 18. Vehicle Access Requirements.

# Site organisation

## Servicing and utilities

*Locate back-of-house activities (such as loading services) underground within the building mass and away from the public realm and public view*

1. Provide access to site servicing and parking at the rear of the building, from a laneway if there is one or from a shared driveway if possible.
2. Minimise the extent of the site area dedicated to servicing and vehicular access, using shared infrastructure and an efficient layout.
3. Avoid freestanding vehicle ramps, loading areas and garbage storage and collection areas or enclosures.
4. Recess, screen and minimise the size of garage doors and service openings that are visible from public streets and public or private open space. Use high-quality doors and finishes.
5. Provide pedestrian and cyclist access to and from parking areas that are clearly visible, well-lit, convenient and easily accessible from the street.
6. Locate ventilation shafts, grates and other above-ground mechanical and site servicing equipment away from footpaths and public and private open space.

### Rationale

Large buildings accommodate two types of activities at-grade: front-of-house activities (such as retail and landscaping) and back-of-house activities (such as parking, loading and servicing). Back-of-house activities include:

- vehicle access, ramps and parking
- drop-off areas
- garbage storage and collection
- loading docks
- vents, utility meters, transformers and other site utilities and servicing infrastructure.

Back-of-house activities are essential for the efficient functioning of a development. Concealing these activities within and behind buildings promotes a safer, more comfortable and attractive public realm and pedestrian environment.

Using the building and high-quality architectural elements and landscape design to screen vehicular access and site servicing also helps mitigate noise, air quality concerns and unattractive views of a building site and on adjacent streets, public and private open spaces and neighbouring properties.

# Building design

## Environmentally sustainable design

*Ensure new buildings reduce their effect on the urban environment, contribute to an enhanced public realm and can adapt to future changes in the economy and environment.*

1. Apply the preferred requirements in *Table 4. Preferred ecologically sustainable design, performance and amenity requirements*.
2. Ensure that development contributes to an increase in urban tree canopy and vegetation cover.
3. Ensure that glazing and material finishes used in development are sufficiently robust and durable to endure in a marine environment.
4. Manage noise and other amenity impacts on residential apartment and accommodation type use and developed adjacent to Latrobe Terrace and the Geelong Railway Station and line.
5. Avoid introducing new use and development, which may create a reverse-amenity conflict with a preferred land use objective of a precinct.

### Rationale

Building performance and amenity are key attributes that relate to a building's energy efficiency and comfort.

Best practice in environmentally sustainable design reinforces Geelong's status as a UNESCO City of Design. Applying environmentally sustainable design principles and practices will help make Central Geelong's building stock more resilient and adaptable.

The internal design of buildings should contribute to an occupants' comfort; improve thermal comfort; reduce the use of mains energy and water supply; maximise sunlight, cross-ventilation and urban greening; and reduce the impacts of noise from adjacent uses.

The adaptive reuse of all building types should be preferred to new development. Ensuring that floor plates and heights in new development suit both commercial and residential uses will help address changing demands for floor space in future.

Development should be designed to withstand the characteristics of Geelong's coastal marine environment, including corrosion, humidity and moisture, to ensure building performance and aesthetic quality are not compromised.

Introducing residential activity into some areas of Central Geelong may limit the ability of established uses (such as bars, restaurants, live music venues and hospitals) to have extended hours of operation. The need for residential population growth must be balanced with the need to maintain and encourage the growth of a vibrant, lifestyle-oriented environment with extended hours of operation.

Table 4. Preferred ecologically sustainable design, performance and amenity requirements

Theme	Preferred requirements
Adaptive reuse	<p>Prioritise building re-use, adaptation, and extension where practical over demolition and rebuilding.</p> <p>Require a minimum floor-to-floor height within the podium of a building of at least:</p> <ul style="list-style-type: none"> <li>• 4 metres at the ground floor level.</li> <li>• 3.5 metres for all levels above the ground floor level and up to the preferred maximum street wall height.</li> <li>• 3.5 metres for car parking structures above ground.</li> </ul> <p>Car parking structures above ground should have horizontal levels to support their adaption to an alternative use.</p>
Building comfort and efficiency	<p>Buildings should ensure the development's orientation, design and layout reduces energy use and makes appropriate use of daylight and passive solar energy.</p> <p>Residential buildings should be designed to deliver a minimum 6 Star Green Star rating across an apartment building and 5 star minimum for individual apartments.</p> <p>Demonstrate how a proposed building will manage stormwater and its reuse on site effectively.</p>
Urban greening and cooling	<p>Encourage roof and vertical gardens in new or refurbished buildings.</p> <p>Encourage buildings to minimise environmental impacts through the selection of building materials, design, and ongoing management.</p> <p>Maximise urban greening by providing canopy trees, ground cover, vertical and roof top vegetation.</p> <p>Encourage buildings to provide a minimum 40 per cent total surface area as green cover.</p>
Coastal environment	<p>Ensure buildings comply with the principles contained in the <i>Siting and Design Guidelines for Structures on the Victorian Coast (2020)</i> and the <i>Marine and Coastal Policy 2020</i>.</p> <p>Use buildings materials that meet appropriate Australian Standards suitable for use within a coastal/marine environment.</p>
Noise	<p>Where new buildings include Accommodation within 80 metres of the Geelong Railway Station, Geelong Railway Line or Latrobe Terrace, the design response should achieve the following noise attenuation levels:</p> <ul style="list-style-type: none"> <li>• Not greater than 35dB for bedrooms, assessed as an LAeq,8h from 10pm to 6am.</li> <li>• Not greater than 40dB for living areas, assessed LAeq,16h from 6am to 10pm.</li> </ul> <p>Developments should incorporate noise attenuation measures and suppression techniques to ensure noise does not unreasonably affect the amenity of public areas and land within the General Residential Zone.</p>
Reverse amenity	<p>Require a new use that is being introduced in an area and that may create conflict with nearby, sensitive land use to demonstrate how any conflict(s) will be ameliorated.</p>



**Above:** The City of Greater Geelong's new office will achieve the highest standards of sustainable design with a 6 Star Green Star Design and As-Built Rating as well as a 5 Star NABERS Energy Rating.



**Above:** Biophilic design seeks to connect building occupants more closely to nature incorporating things like natural lighting and ventilation, natural landscape features and other elements for creating a more productive and healthy built environment for people.

# Building design

## The podium - Street wall height

*Design the base of a building to fit harmoniously within the existing context of neighbouring building heights, respect the scale and proportion of adjacent streets and interface with public open space.*

1. Street walls should help create human-scale streets that respond to street and laneway widths and the preferred maximum building height.
2. Apply the preferred maximum street wall heights set out in *Figure 19. Preferred maximum street wall heights and prominent corners*
3. Where appropriate, street wall heights should vary in a manner that reflects the prevailing urban grain of the existing subdivision pattern of a given area.
4. Sites that are subject to a Heritage Overlay should retain the street wall of the existing heritage building, including where the street wall is greater than the nominated street wall height.
5. Proposed development on sites that adjoin heritage buildings and places should establish a street wall height that appropriately transitions between two heritage buildings and/or is in proportion to that of the adjoining heritage building frontage.
6. Blank walls that are visible from the street should be avoided.
7. Provide a minimum first-floor height of 4 metres, measured floor to floor.

### Rationale

The street wall height refers to the height of the building at the street edge, excluding the upper levels set back above the street wall. The prominence of the podium shapes pedestrians' experience of the building and the collective streetscape.

An appropriately scaled street wall at a human scale contributes to an inviting and sunlit street with a sense of enclosure while offering clear views of the sky.

A range of maximum street wall heights is applied based on several existing factors and balanced with a preferred future use and development scale envisaged for that place. These factors include:

- the preferred land use objective for the precinct
- the existing street wall height and the overall scale of buildings
- the broader streetscape environment and urban character that has formed over time
- the heritage significance of a place, area or building
- the need to preserve access to sunlight in primary streets, laneways and existing and proposed open spaces.

Preferred maximum street wall heights have been applied to define the scale and enclosure of the street and are set out in *Figure 19. Preferred maximum street wall heights and prominent corners*.



In principle, buildings should be built to the front lot line to reinforce a mixed-use urban character. Requirements for a street wall located behind a landscaped setback are set out in *Figure 19*. Exceptions include:

- where there is an existing landscaped setback setting, the street wall setback should be in proportion to existing setbacks
- where institutional uses are proposed, an appropriate setback should be set to respond to such uses and provide for landscaping and placemaking opportunities
- where a site is located on the periphery of Central Geelong, there should be an appropriate landscaped setback to provide a distinct change between the street and urban environments.

A generous ground-floor height gives prominence to the street level, establishes a clear presence for retail and increases the visibility, marketability and utility of ground-floor space. A minimum floor-to-floor height of 4 metres also accommodates the vertical clearance required for loading spaces and for manoeuvring trucks within the rear of the building, which is essential to the viability of retail and many other commercial activities.

Where appropriate, taller first-floor heights for at-grade residential units are encouraged to provide continuity in the base building and permit adaptability and long-term flexibility, including possible future conversion to retail or commercial uses.

The floor-to-floor heights of adjacent heritage properties or lower-scale buildings that are not anticipated to change should be reflected in the design and articulation of the base building to promote integration and fit with the context.

Provide an appropriate transition in street wall height between heritage buildings



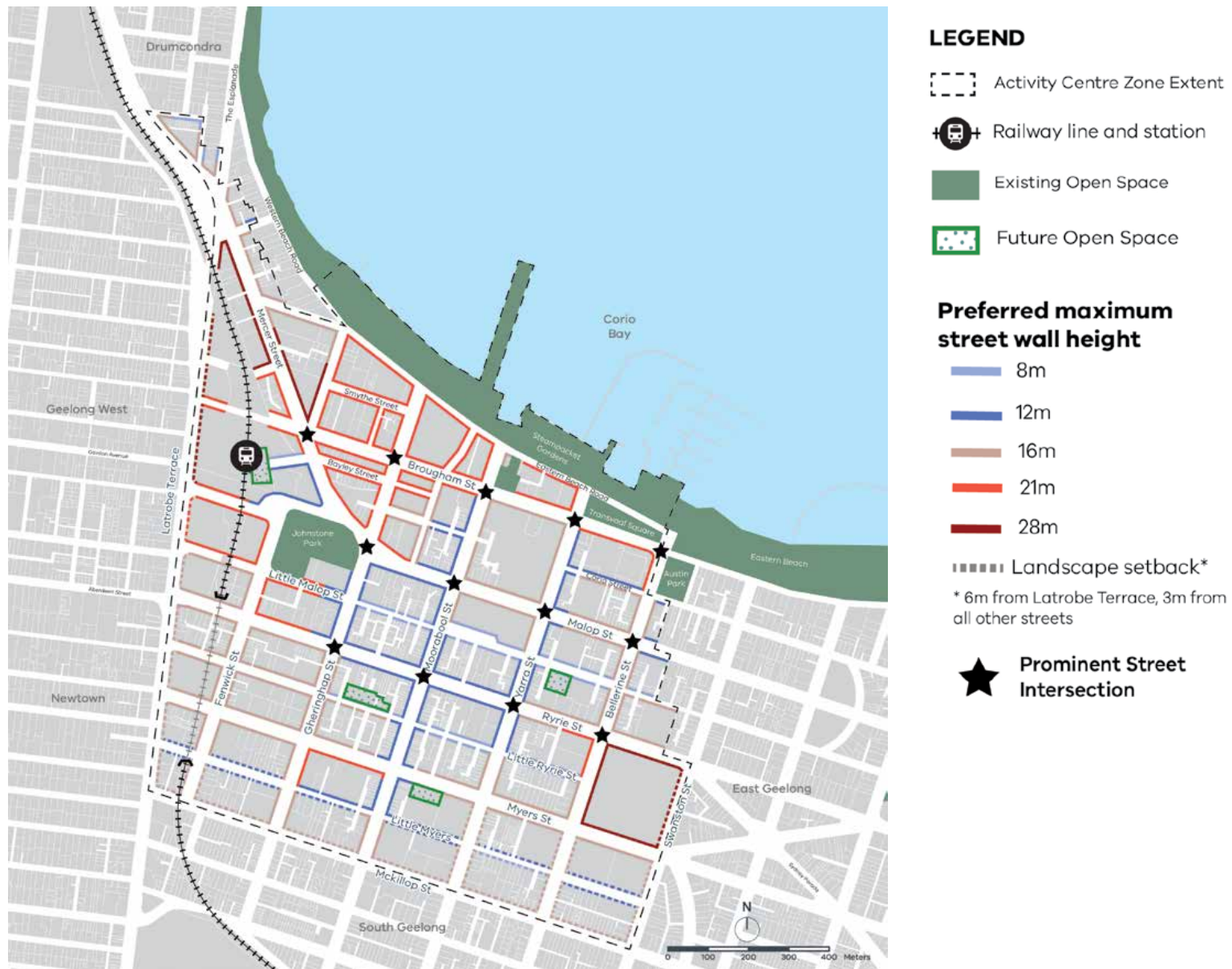
Street wall height which is taller than or dominates heritage buildings should be avoided



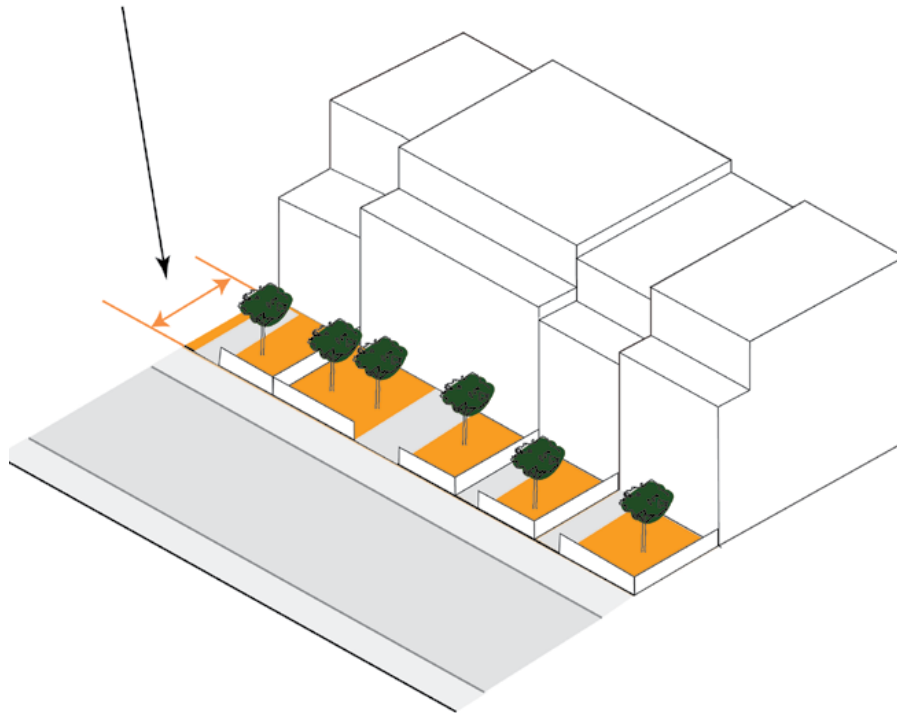
**Above:** Providing a transition to the street wall height wall between heritage buildings provides appropriate scale respecting the three dimensional forms of respective heritage buildings.



Figure 19. Preferred maximum street wall heights, landscaped setbacks and prominent corners



Provide a 6m landscaped setback along Latrobe Terrace.  
Provide a 3m landscaped setback along all other nominated streets.



**Above:** Landscape setbacks from the street provide a separation of built form from the busy streetscape environment of Latrobe Terrace and Mckillop Street. Other nominated streets within southern areas of Central Geelong have a consistent character of building setbacks that should be retained.



**Above:** Latrobe Terrace (pictured) is a major transit and freight corridor servicing Geelong and its broader region. It also acts as a gateway into Central Geelong. Establishing a landscaped setback of 6 metres provide a buffer from noise and will improve the appearance and interaction at the periphery of the Central Geelong activity centre area with this major road.

# Building design

## The podium - Street wall heights on corner sites

*Building corners should be designed to be articulated and expressed volumetrically, addressing both streets and façades.*

1. Apply the following approach to establishing a preferred overall maximum street wall height for a corner location:
  - a. on a 'prominent street intersections' identified in *Figure 19. Preferred maximum street wall heights and prominent corners*, apply the higher of the preferred street wall height for that corner to a maximum length of 30 metres on both street frontages
  - b. on all other corners, apply the average of the street wall height on that corner to a maximum length of 30 metres on both adjoining street frontage sides.
2. Overall design should acknowledge the importance of a prominent street intersection by including a corner entrance and providing an active frontage overall.
3. An entrance to a ground-floor commercial or retail space should be placed in the most prominent position, while an entrance to a residential space should be located along the façade on either street.
4. A corner entrance should be integrated into the building's overall design, employing a variety of design techniques and materials to provide interest and difference for each street corner.

### Rationale

Corner buildings are important to Central Geelong. They are highly visible and where people are most likely to congregate. Prominent corner buildings can also act as navigation markers. These are identified on *Figure 19. Preferred maximum street wall heights*.

On corner lots, articulation of the podium should acknowledge its important location through corner entrances, chamfering (and associated public space), and/or other architectural features. Mixed-use buildings with retail at grade should incorporate vestibules, frequent building entrances, canopies and structural overhangs to provide weather protection for the length of the corner on both street frontages.



# Building design

## The podium - Side and rear setbacks

*Provide sufficient distances between buildings and neighbouring sensitive uses to allow equitable access to sunlight and daylight to windows and habitable rooms of existing buildings and to future development on adjoining sites.*

1. Provide podium-level side and rear setbacks equivalent to or greater than the requirements in *Figure 20. Ground level podium side and rear setback requirements when not on the boundary*, as measured from either the adjacent property boundary or the centre of an adjoining laneway.
2. Where a development is proposed adjacent to land within a residential zone, apply the requirements of ResCode Standard B17 of clause 55.04-1 'Side and rear setback objectives' in Greater Geelong Planning Scheme to reduce the amenity impact of the development.
3. Encourage appropriate landscaping responses for set-back areas.
4. Avoid encroachments of balconies, servicing equipment, stairways, ramps or other structures within the established setback.

### Rationale

Appropriate setbacks provide equitable access to sunlight and daylight to windows and habitable rooms, increase privacy in buildings and outlooks from them, and minimise the need for screening.

Buildings should be built to the property boundary. Where buildings are not built to a property boundary a setback from the boundary should be applied in accordance with Figure 20. In principle, greater separation is required as buildings increase in height and bulk. This will ensure that building separation is provided above 16 metres or the nominated maximum street wall height if it is greater than 16 metres.

Where land within the Activity Centre Zone shares a common property boundary with land in a residential zone, application of ResCode Standard B17 is the preferred approach to managing the height and scale of a building to respond to the existing and/or preferred neighbourhood character and to minimise amenity impacts on adjoining, residentially zoned land

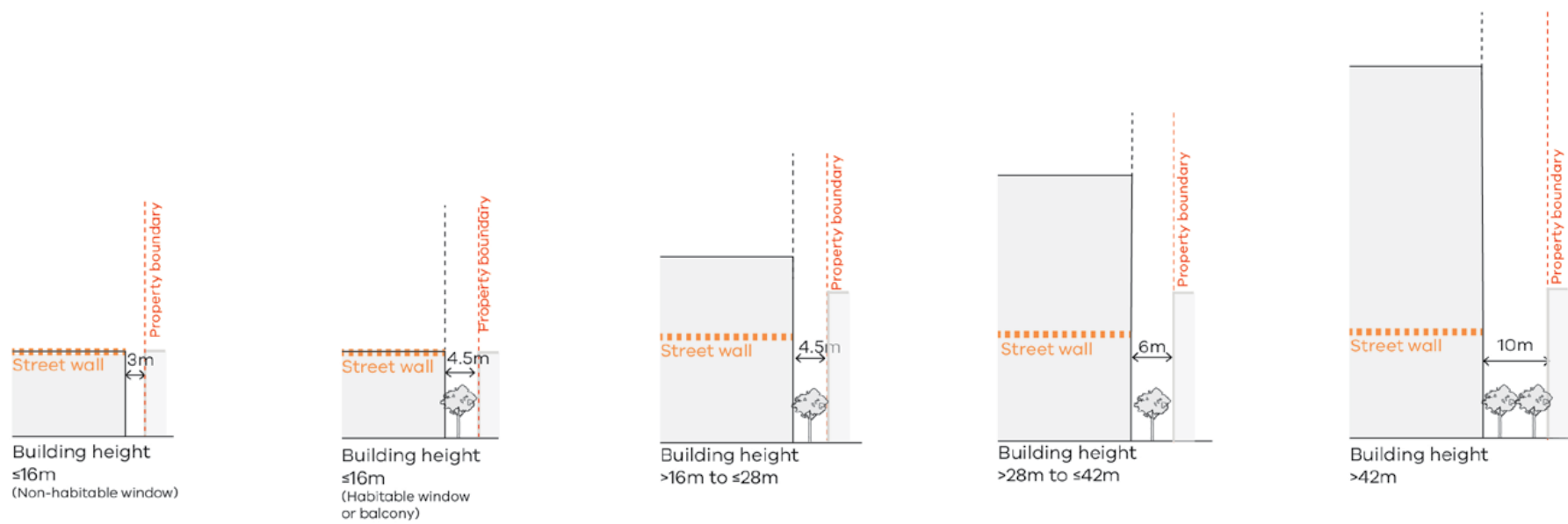


Figure 20. Ground level podium side and rear setback requirements when not on the boundary

# Building design

## Above the podium - Tower floor plate size and shape

*Limit the tower floor plate to 1,000 square metres or less per floor, including all the built area within the building, excluding balconies.*

1. Apply a preferred maximum floor plate size of not more than 1,000 square metres per tower.
2. Encourage slender tower design with a length-to-width ratio of at least 2:1.
3. Organise, locate and articulate the tower floor plate to:
  - a. minimise shadow impacts and negative wind conditions on surrounding streets, parks, open space and properties
  - b. minimise the loss of sky view from the public realm
  - c. allow for the passage of natural light into interior spaces (with, for example, shallow rather than deep floor plans)
  - d. create architectural interest and visually diminish the overall scale of the building's mass
  - e. present an elegant profile for the skyline of Geelong.
4. Provide greater tower separation, setbacks and stepbacks proportionate to increases in tower floor plate size or height to mitigate resultant wind, shadow and sky view impacts.

### Rationale

The size and shape of the tower floor plate work together with the height and placement of the tower to determine the overall three-dimensional massing of a tall building and its visual and physical impact on surrounding streets, parks, open space and properties. The tower floor plate size includes all the built area within the building, measured from the exterior of the main walls at each floor above the base building, excluding balconies.

When adequately separated, slender, point-form towers with compact floor plates cast smaller, faster-moving shadows, improve access to sky views, permit better views between buildings and through sites and contribute to a more attractive skyline. A smaller floor plate can also make interior climate control more energy-efficient and increase daylighting within the tower which is an important contribution to sustainability, residential liveability and workplace productivity.

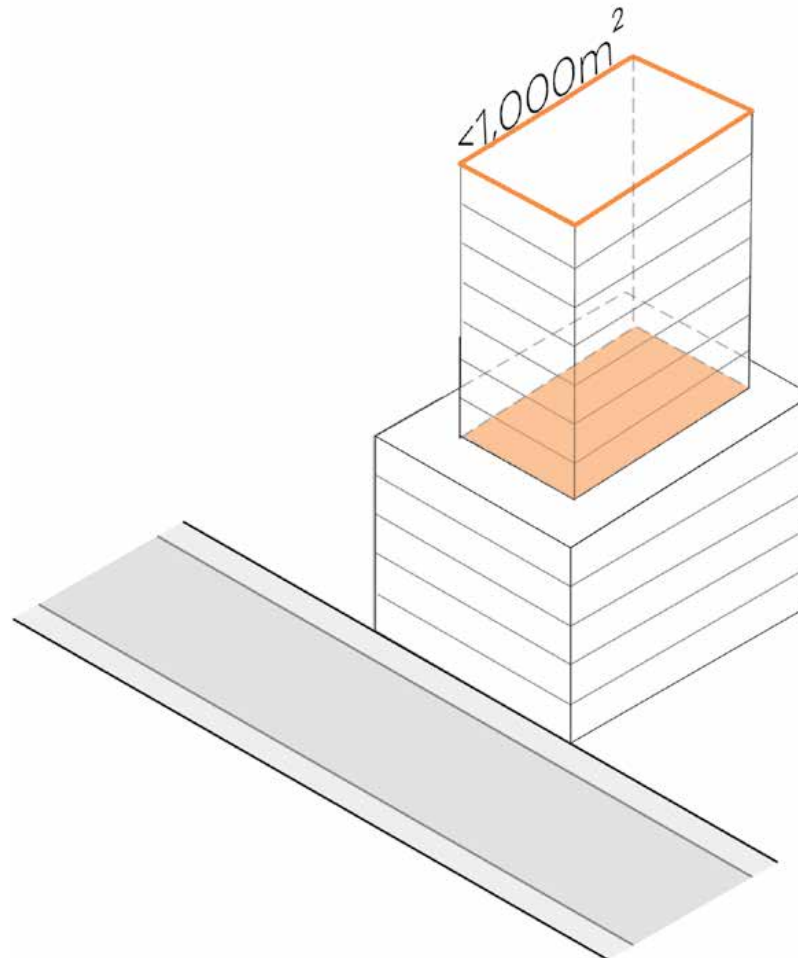
The tower floor plate size and shape should be determined together with the site dimensions and required tower separation distances, setbacks and stepbacks. A floor plate smaller than 1,000 square metres may be necessary to achieve adequate tower setbacks and spacing on a site.



On a site-specific basis where adequate tower separation, setbacks and stepbacks are achieved, flexibility in the maximum floor plate size may be considered for the tower or a portion of it:

- to make the interior layout of non-residential uses (such as commercial-only buildings, commercial-only floors of mixed-use buildings, institutional buildings and hotels) economically viable
- to accommodate modest increases in additional servicing and structural requirements for very tall buildings (such as residential or mixed-use buildings greater than 42 metres in building height).

Any increases in tower floor plate size require exceptional design attention to be given to the shape and articulation of the tower to diminish the overall scale and impact of the building mass. Greater tower separation and setbacks and stepbacks proportionate to increases in a building's size are also effective ways to mitigate the resultant wind, shadow and sky view impacts on surrounding streets, parks, open space and properties.



**Above:** The maximum floor plate size of 1,000 sqm includes all built areas which includes all dwellings, offices, indoor amenity facilities, elevator cores, storage spaces, stairwells and hallways.

# Building design

## Above the podium - Tower setbacks

*Recess towers above a podium to reduce visual impacts and allow the podium to be the defining element for the site and adjacent public realm.*

1. Apply the requirements set out in *Table 5. Mandatory and preferred minimum upper setbacks above the street wall height* if the building is built to the property boundary fronting a street.
2. Provide side and rear upper-level setbacks equivalent to, or greater than, the requirements in *Figure 22. Preferred minimum side and rear setbacks from a property boundary above the preferred street wall height*.
3. Towers should be setback above the preferred maximum street wall height an additional distance to ensure compliance with the overshadowing requirements outlined in *Table 8. Overshadowing requirements for public open spaces* and *Table 9. Overshadowing requirements for streets and laneways*.
4. Coordinate a tower's placement with other towers on the same block and adjacent blocks to maximise access to sunlight and sky view for surrounding streets, parks, open space and properties.
5. Where multiple towers exist on a site, they should be arranged to provide a gradual and appropriate transition in height to adjacent, established, or planned development, with this transition being reflected across the entire site.
6. Tower stepbacks are encouraged and may be required for tall buildings to fit harmoniously within an existing context, including on sites that contain, or are adjacent to, heritage properties or have overshadowing requirements.

### Rationale

The recession of tower elements above the podium street wall shapes the interaction with and experience of a building and the collective streetscape when experienced at ground level. To optimise this interaction and experience, a development should establish appropriately scaled upper-level setbacks that incorporate a single setback above the podium street wall to avoid a tiered, 'wedding-cake' form.

For sites subject to a Heritage Overlay, a minimum setback of 6 metres must be provided for any upper-level tower elements above an existing heritage building. This is to ensure that the three-dimensional form of the heritage building is retained and to avoid façadism as an outcome.

Podium elements should adopt the lower street wall height where a range is specified. For example, for a specified range of 21 metres to 28 metres, the preferred maximum street wall height of 21 metres should be applied for buildings on a site above 42 metres. Non-podium-tower building typologies may adopt the higher street wall where a range is specified

Table 5. Mandatory and preferred minimum upper setbacks above the street wall height from a street

Overall building height	Mandatory minimum front setback above the preferred maximum street wall height in a Heritage Overlay	Preferred minimum front setback above the preferred maximum street wall height
Equal to or less than 42 metres	6 metres minimum on the main street frontage	6 metres
Greater than 42 metres	6 metres minimum on the primary street frontage	10 metres

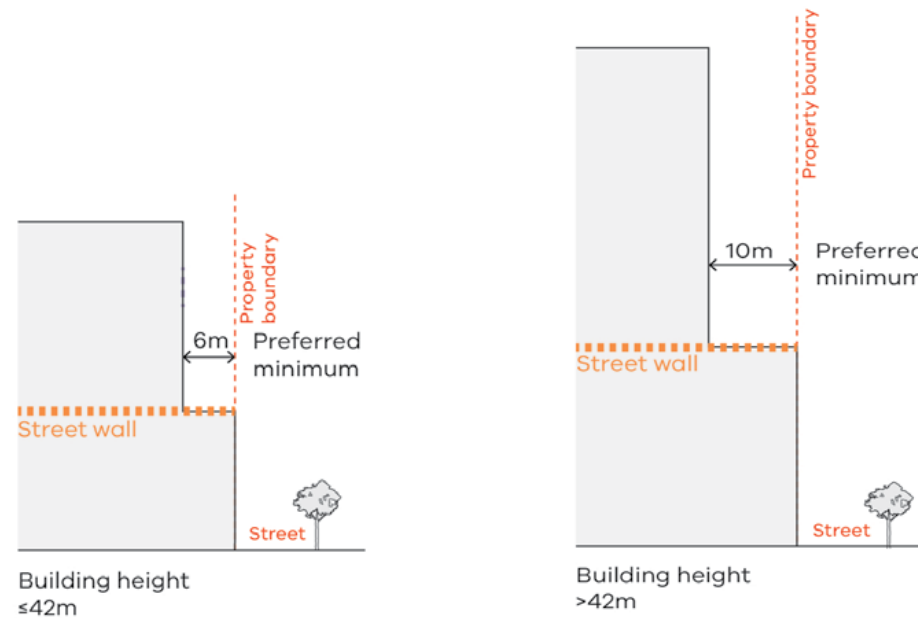
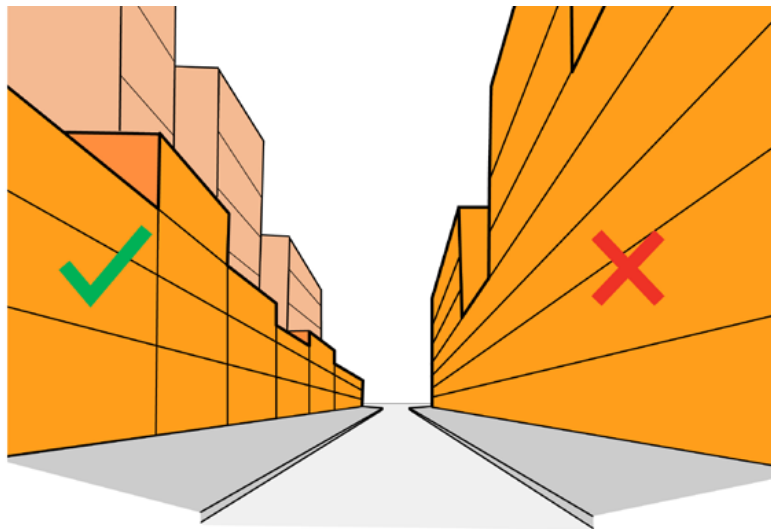


Figure 21. Preferred minimum setbacks above the preferred maximum street wall from a street



**Left:** Applying defined setbacks for built form above the podium will provide for development at ground level that is of a 'human scale' and will help preserve key views to Corio Bay and other landmarks within Central Geelong.

Street walls that are too high or tower elements built too close to the street will dominate views within and down major streets creating uncomfortable and unappealing conditions of pedestrians.

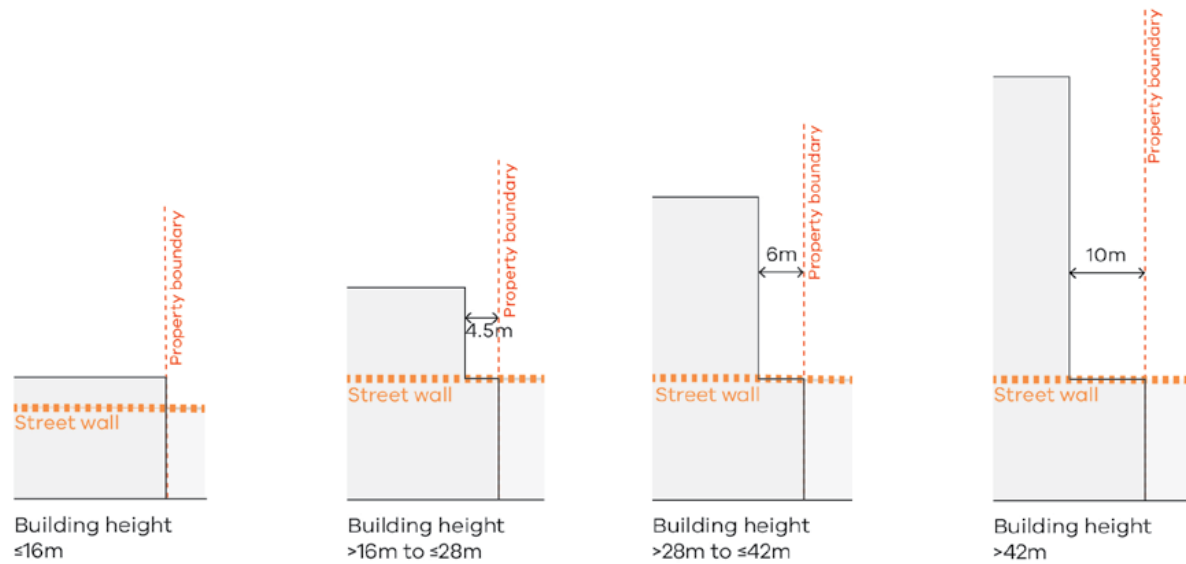


Figure 22. Preferred minimum side and rear setbacks from a property boundary above the preferred street wall height..



Work Safe

# Building design

## Above the podium - Tower separation distances

*Provide adequate space and distance between tall buildings to provide a sense of space and reduce impacts on neighbouring buildings and the public realm.*

1. Provide tower separation distances, measured from the façade of each building (including balconies), of:
  - a. 12 metres for buildings with an overall height of 42 metres or less
  - b. 20 metres for buildings with an overall height of greater than 42 metres.
2. Where taller buildings (above 42 metres) or larger tower floor plates (above 1,000 square metres) are proposed, provide greater setbacks and separation distances proportionate to increases in building size and height.
3. Coordinate tower setbacks and separation distances with other towers on the same block and adjacent blocks to maximise access to sunlight and sky views for surrounding streets, parks, open space and properties.
4. Where possible, apply creative solutions (such as offset towers/views, non-parallel walls and tapering or curved tower forms) to increase the actual or perceived tower separation distances if access to sky views is maintained and adverse wind and shadow conditions are minimised.

### Rationale

Adequate tower separation distances from property lines and from other towers are an essential aspect of tall building design. The placement of towers should minimise negative impacts on the public realm and neighbouring properties (such as adverse shadowing, pedestrian-level wind and blocking of sky views) and should maximise the environmental quality of building interiors, including daylight access, natural ventilation and privacy for building occupants.

Separation distances greater than 20 metres for buildings above 42 metres in overall height may be necessary to ensure any tower element fits harmoniously within an existing or planned context. For buildings below 42 metres, a separation distance of 12 metres is appropriate.

The minimum separation distances are established to ensure tall buildings achieve the following objectives to protect sky views and privacy and ensure adequate daylight.

- **Sky view:** The ability to retain adequate sky view in between building masses is essential to maintain the character, usability and quality of streets, parks, open space and neighbouring properties. Lack of sky view can also be to the detriment of the microclimate and sense of pedestrian scale at-grade.

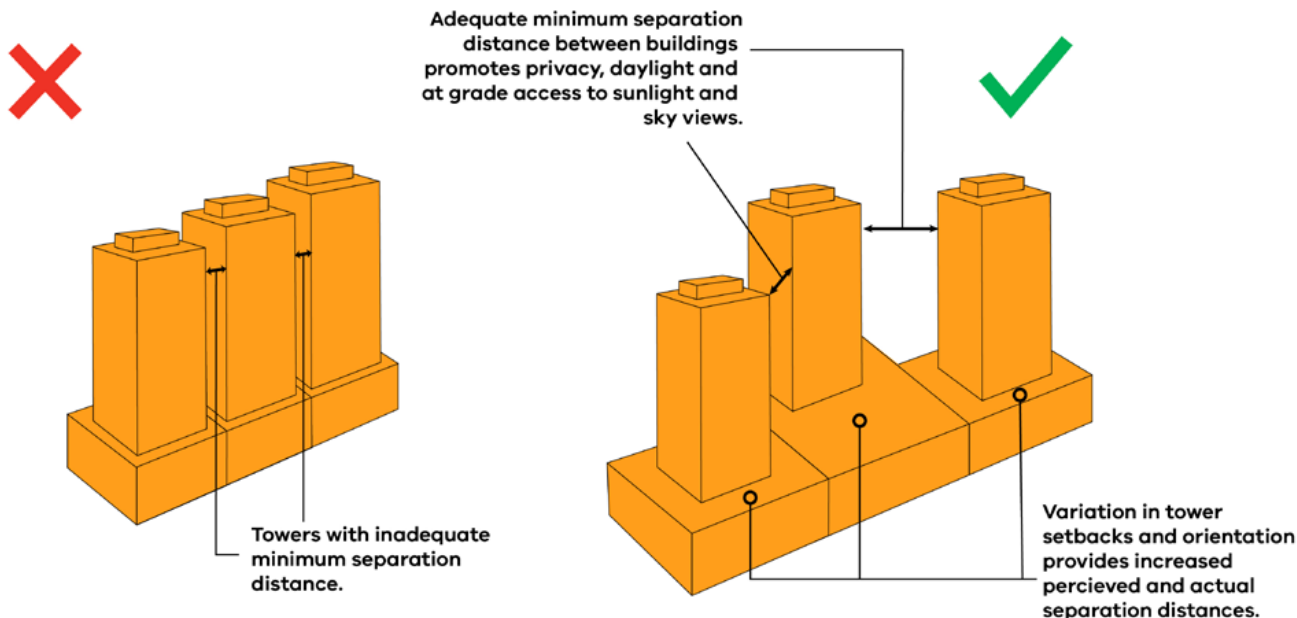
- **Privacy:** Privacy objectives, particularly for residential units, are achieved when tower orientation, appropriate facing distances and setbacks combine to mitigate overlook between the windows or balconies of one building and those of another.
- **Daylight:** Access to natural light in the building interior is an important component of residential liveability, workplace productivity and sustainable

building practice. An adequate level of daylighting is achieved for residential buildings when natural light reaches the main living space for part of the day. Tall buildings with small floor plates and adequate separation provide enhanced opportunities for daylighting.

Since tall building development is typically evaluated on a site-by-site basis, it is important to understand the cumulative effect of the proposed tall building

within the context of other tall buildings. Clusters of towers can incrementally affect the streetscape and nearby open space and residential neighbourhoods.

Even if tall buildings near other tall buildings meet the minimum required separation distances, towers should also be set back, oriented and shaped to increase the actual and perceived distances between adjacent building elevations.



*Left: Adequate minimum separation distances should be measured from the closest building face to building face. Providing adequate space between tower elements helps provide access to sunlight and sky views from the surrounding public realm and neighbouring buildings and will also improve privacy and daylight within buildings.*

# Building design

## Above the podium -Tower orientation and articulation

*Organise and articulate the towers of tall buildings to promote excellence, innovation and sustainability.*

1. Orient towers to improve building energy performance, natural ventilation and access to sunlight.
2. Present the longer axis of a tower in a north-south orientation where possible to maximise sky views, increase space between towers and reduce overshadowing.
3. Variation in the design and articulation of each tower façade is encouraged to provide visual interest and to respond to design opportunities and differing facing conditions within the adjacent context.
4. When multiple towers are proposed, stagger the tower heights to create visual interest within the skyline, mitigate wind and improve access to sunlight and sky views.
5. Where possible, provide internal flexibility within the tower to accommodate changing floor layouts and uses over time.
6. Articulate towers with high-quality, sustainable materials and finishes to promote design excellence, innovation and building longevity.

### **Rationale**

The orientation and articulation of the tower are critical to the overall perception of the three-dimensional massing of a tall building, its physical effect on adjacent areas and its visual impact on the skyline. Designs that reduce the appearance of the tower's overall bulk and present a slender, point-tower form in the skyline are encouraged.

Orienting and articulating the tower in relation to the seasonal paths of the sun across the sky, combined with the arrangement of internal spaces, can greatly improve natural daylighting, liveability and energy efficiency. When making design decisions, it is important to balance building performance objectives with the need to maintain sky views and minimise adverse wind and shadow impacts on the public realm and surrounding properties.

The tower portion of a tall building is often the most visible and the most resource-intensive part of the development. Internal flexibility, operable windows, sunshades and other high-quality, sustainable materials, finishes and construction methods foster a building's sustainability and longevity.



# Building design

## Above the podium - Rooftop design

*Design the top of a tall building to make an appropriate contribution to the quality and character of the city skyline.*

1. Provide allowances in the overall building height to accommodate rooftop mechanical equipment (such as plant rooms, lift overruns and solar collectors) provided the equipment:
  - a. is no more than 3.6 metres above the preferred maximum building height
  - b. occupies no more than 50% of the roof area
  - c. is stepped back on all sides no less than 3 metres from the edge of the building is screened from view.
2. Design the upper floors of a tall building to clearly distinguish its top from the tower, to further reduce the building profile and achieve a distinct skyline. This may include stepbacks, material variations and unique articulation.
3. Where possible, outdoor amenity space should be included within the tops of buildings, including balconies, patios, terraces, rooftop gardens and pools.

### Rationale

An appropriate design for the top of a tall building is influenced by, among other things, its location, height, built form composition, architectural expression and overall fit with the city's skyline.

Most tall buildings form part of the urban backdrop, which frames existing landmarks and public open space. The tops of these buildings should reinforce their supporting role and subtly integrate with the overall tower design.

A small number of tall buildings (such as those terminating a view) could benefit from a signature tower top to strengthen the building's identity as an orienting landmark.

In all instances, rooftop mechanical and telecommunications equipment and signage must be well-integrated into the total building design to avoid detracting from the form and elegance of the top.

When decorative lighting of the tower's top or other key architectural features is included to enhance the building's design and presence at night, the lighting does not have to be turned on for the whole night or whole year to be effective.

# The pedestrian realm

## Street activation

*Support a vibrant, active and safe pedestrian environment with a mix of techniques and approaches to activating streetscapes, recognising differences in precincts and types of land use.*

1. Apply the preferred street activation according to the land use in *Table 6. Preferred street activation, by land use*.
2. Apply the preferred glazing requirements for active frontages in *Table 7. Preferred glazing requirements* and *Figure 23. Active frontages and glazing requirements*.
3. Encourage activity along street frontages consistent with *Figure 23. Active frontages and glazing requirements* to support the mix of land uses identified in each precinct's objectives.
4. For buildings on the Victorian Heritage Register and/or within a Heritage Overlay, the protection of heritage elements takes precedence over glazing requirements.
5. Vehicle ingress and egress, loading facilities and other building services should not be located on active frontages.

## Rationale

The interaction between what occurs within a building and its immediate streetscape environment is what provides an attractive, active environment for pedestrians during the day and evenings.

The building façade should provide an architectural expression that relates to its surroundings and include materials and elements that can be viewed and appreciated at the speed and proximity of pedestrians.

Although the articulation and transparency of all faces of a building are important, those fronting streets, parks and open space are the most important. At street level, clear, unobstructed views into and out of buildings enrich the urban experience for pedestrians and building occupants alike. Transparency in the building façade adds visual interest, contributes to a sense of liveliness on the street and improves safety through natural surveillance.

A high degree of visual and physical connection, including multiple entrances and storefront windows, supports active, street-related commercial and retail uses. Glazing — appropriately proportioned windows, doors and other architectural techniques — at the ground plane increases pedestrians' visual engagement with and interest in retail and hospitality uses.

Figure 23. Active frontages and glazing requirements



Table 6. Preferred street activation, by land use.

Use type	Preferred requirements
All uses	<p>Establish a minimum ground floor height of 4 metres from average grade to provide a clear street presence to increase its visibility, marketability, adaptability and utility.</p> <p>Use high-quality architectural and landscape design to emphasise primary entrances.</p> <p>Avoid blank walls, high fences, service areas, car parks and garage doors fronting streets and active laneways.</p> <p>Ensure recesses in ground floor walls are not more than 0.3 metres deep to ensure they cannot be used as places to hide or entrap.</p> <p>Provide high levels of interaction between what occurs within a building and its immediate streetscape environment by providing regular entrances and operable windows.</p> <p>Position building entrances and transparent windows on all façades facing streets, parks, and open space.</p> <p>Avoid the use of security grilles or mesh. When used, security grilles should be visually permeable or transparent and are encouraged to be internally mounted.</p> <p>Façade designs should incorporate appropriate lighting to provide safety of entrance and sense of security at night.</p> <p>Apply the principles of Crime Prevention Through Environmental Design and Universal Design.</p> <p>Provide direct, universal access, flush with the footpath.</p>
Office and retail premises	<p>Provide an entrance to each ground floor retail unit, which is identifiable and directly accessible from the public footpath.</p> <p>Provide continuous canopies for weather protection with sufficient overhang space while allowing for street canopy trees to be planted.</p>
Accommodation	<p>Provide residential entries separate from commercial entries.</p> <p>Provide individual entries from the street to dwellings or home offices at ground level where practicable.</p> <p>Filter and screen views into private dwelling units but ensure views to streets and open space are maintained for natural surveillance.</p> <p>Provide balconies and operable windows within the street wall and orient habitable rooms towards the street and laneway to increase passive surveillance opportunities.</p> <p>Where relevant, provide habitable rooms orientated towards abutting open space.</p>

Table 7. Preferred glazing requirements for active frontages

Active frontage type	Preferred requirements
Active retail street	Provide transparent glazing up to 2.5 metres in height, of not less than 80 per cent of the width of the frontage, excluding any column or solid plinth or base.
Active street	Provide transparent glazing up to 2.5 metres in height, of not less than 40 per cent of the width of the frontage, excluding any column or solid plinth or base.
Active laneway (existing and proposed)	Maximise the amount of transparent glazing to 2.5 metres in height, excluding any column or solid plinth or base.

**Right:** An active streetscape is created where the front facade of buildings, including the main entrance, faces and opens towards the street.

Ground floors may accommodate uses such as cafes, shops or restaurants, or other uses where there is frequent a movement of people in and out of an establishment.

A building's upper floor windows and balconies may also contribute to the level of active frontage. Active frontages can provide informal surveillance opportunities and often improve the vitality and safety of an area.

Image Credit: [www.issuu.com](http://www.issuu.com)



# The pedestrian realm

## Overshadowing of public open space

*Provide year-round access to sunlight for existing and designated new open space to provide a high-quality public realm environment that entices people to visit Central Geelong and enjoy its open space.*

1. Apply mandatory overshadowing controls to protect existing and future open space measured at the winter solstice in accordance with *Table 8. Overshadowing requirements for existing and future public open spaces* for areas designated in *Appendix D: Existing and future open space locations*.
2. Building design must demonstrate via appropriate modelling or another technique that no part of a new building overshadows any part of a space shown in *Appendix D* in accordance with the requirements in *Table 8*.
3. An overshadowing assessment should consider the main form and structure of a building that will be the primary source of overshadowing. Additional overshadowing from incidental elements such as canopies, kiosks artworks, screens can be excluded from the assessment in addition to any requirements outlined in *Table 8*.

### Rationale

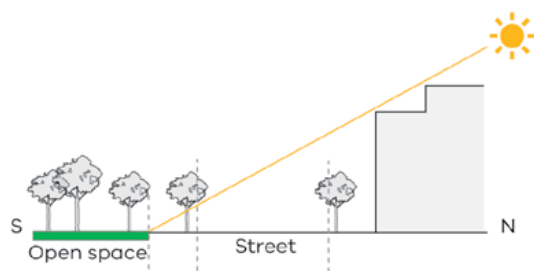
While Central Geelong has a magnificent waterfront, there is little public open space south of Malop Street: Johnstone Park is the only public open space within Central Geelong south of Mercer Street – Malop Street.

Protecting year-round access to sunlight for open spaces is essential for the environmental health of the vegetation in open spaces and for their use and enjoyment by residents, workers and visitors, now and in the future.

Protecting the locations identified in *Appendix D* is also essential to create places in future where people can meet, recreate or simply escape the internal life of a building.

Table 8. Overshadowing requirements for existing and future public open spaces

Open space	Mandatory requirement	Time of year
Johnstone Park	No additional overshadowing	10am – 3pm 22 June
Austin Park		
Customs Park		
Transvaal Square		
Future open spaces	No additional overshadowing beyond a shadow that would be cast by a wall on a boundary of not more than 8 metres.	10am – 3pm 22 June
Geelong Station future plaza /forecourt		



**Above:** Avoiding “additional overshadowing” means any shadow cast by an existing building and/or by incidental elements such as canopies, kiosks, and artworks.



**Above:** For proposed future open space and the Geelong Station future plaza/forecourt, allowance is provided for additional overshadowing that is cast from an existing or proposed 8 metre wall built on the boundary to that open space

# The pedestrian realm

## Overshadowing of key streets and laneways

*Prevent overshadowing of key streets and laneways to provide a pleasant pedestrian environment at the times of the year when the sun is lower in the sky.*

1. To protect key streets and laneways from overshadowing at the September equinox, apply the mandatory controls set out in *Table 9. Overshadowing requirements for streets and laneways* to the areas shown in *Figure 24. Central Geelong overshadowing of streets and laneways*.
2. Building design must demonstrate via appropriate modelling or another technique compliance with the requirements in *Table 9*.
3. An overshadowing assessment should consider the main form and structure of a building that will be the primary source of overshadowing. Additional overshadowing from incidental elements such as canopies, kiosks artworks, screens can be excluded from the assessment in addition to any requirements outlined in *Table 9*.

*Table 9. Overshadowing requirements for streets and laneways*

Streets/laneway	Mandatory requirement	Between
Primary north-south street	No additional overshadowing on the opposite side of the street to either an east or west footpath measured 6 metres from the property boundary.	11 am – 2 pm 22 September
Primary east-west street	No additional overshadowing to a south footpath measured 6 metres from the property boundary.	11 am – 2 pm 22 September
Secondary north-south street	No additional overshadowing cast from buildings on the east side of Mercer Street on to the footpath on the west side of Mercer Street measured 6 metres from the property boundary.	11 am – 2 pm 22 September
Narrow street/laneway A	No additional overshadowing beyond the shadow that would be cast by a street wall height of not more than 8 metres.	11 am – 2 pm 22 September
Narrow street/laneway B	No additional overshadowing beyond the shadow that would be cast by a street wall height of not more than 12 metres.	11 am – 2 pm 22 September

### Rationale

The amenity of Central Geelong’s network of streets and laneways is an essential element in a vibrant, active streetscape environment, particularly the amount of sunlight primary streets and laneways receive in the middle of the day at the times of the year when the sun is lower in the sky.

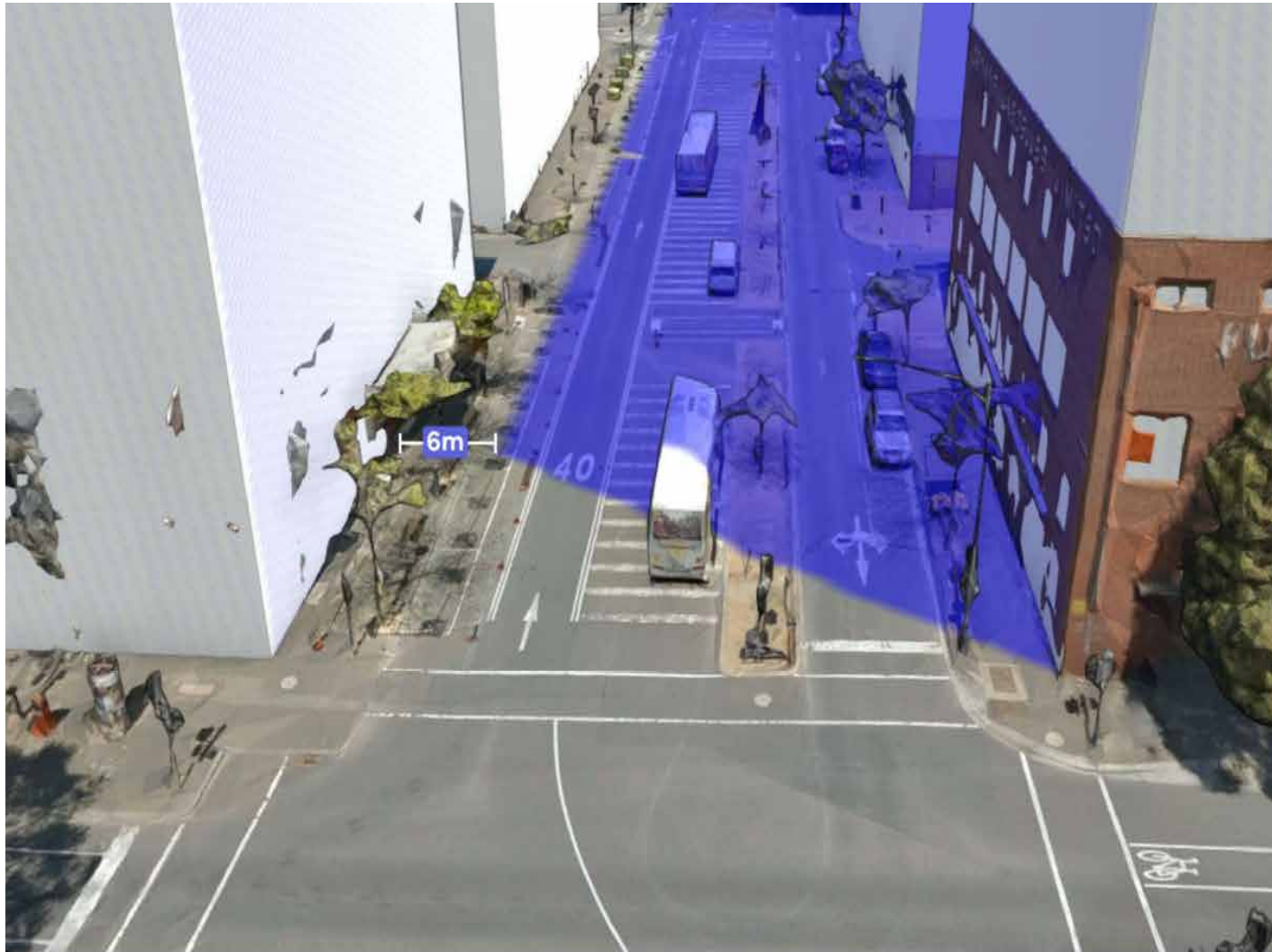
Central Geelong’s grid of streets and laneways is oriented about six degrees north-east of true north. This northerly aspect, the generous street widths and limited development height mean there are considerable opportunities to preserve and protect access to sunlight within defined aspects of the street environment.

Protecting access to sunlight for streets, for at least the warmer half of the year, is essential to support their social and economic uses and vegetation, so they are welcoming places now and in the future.

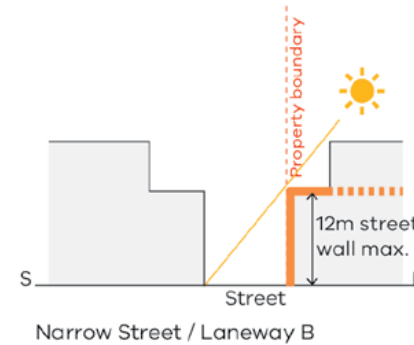
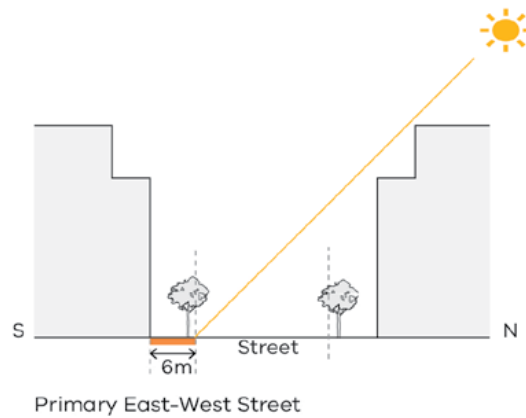
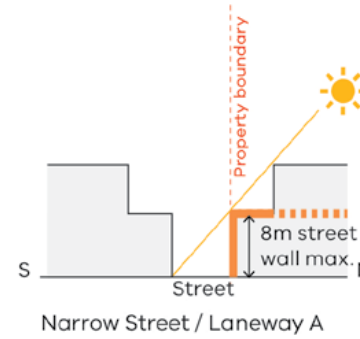
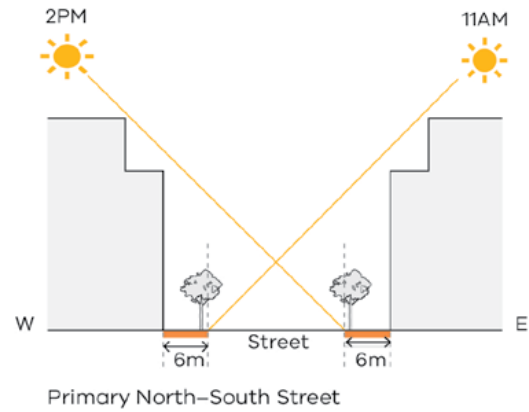
Sunlight on the street will be protected within 6 metres of the property boundary, limiting overshadowing by buildings on the opposite side of the street.

Little Malop Street (and other high-activity pedestrian links — narrow streets and laneways — located mostly within the Retail Core Precinct) will be protected from overshadowing by limiting street wall heights to 8 metres on the northern side of laneways. Other identified narrow streets and laneways will be protected with a maximum street wall height of 12 metres to limit overshadowing.





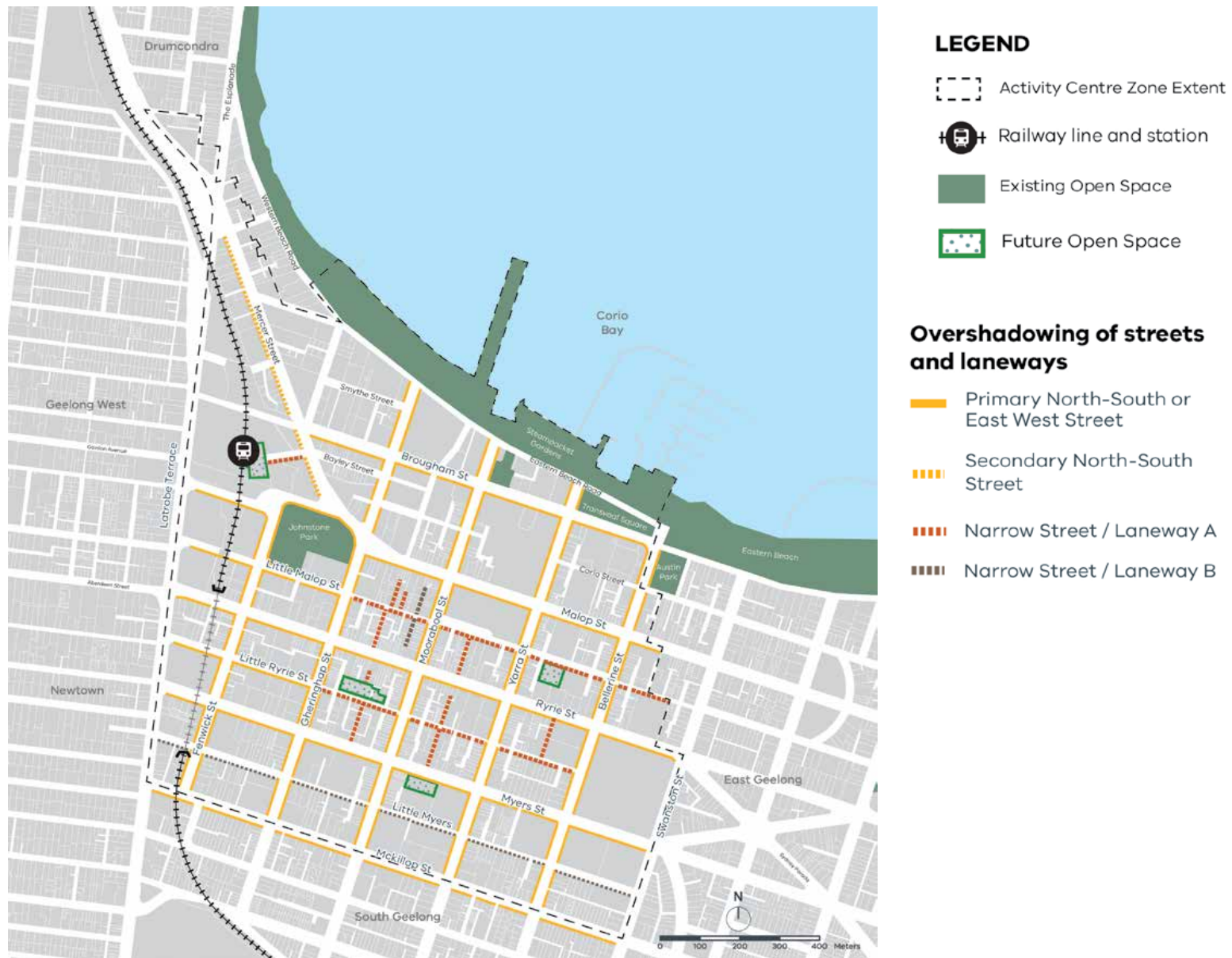
**Above:** Detailed 3D GIS based modelling has informed the development of all planning controls. Ensuring sunlight can reach the ground during the spring/autumn equinoxes will support the amenity of pedestrians at street level and provide opportunity for street plantings to flourish.



**Above:** For land adjacent to a primary north-south or east-west street in Figure 24, the extent of overshadowing is measured from the property boundary to a distance of 6 metres at the spring equinox between 11am and 2pm.

**Above:** For land adjacent to a Narrow Street/Laneway A or B in Figure 24, no additional overshadowing beyond the shadow that would be cast by a street wall height as specified above measured at the spring equinox between 11am and 2pm.

Figure 24. Central Geelong overshadowing of streets and laneways

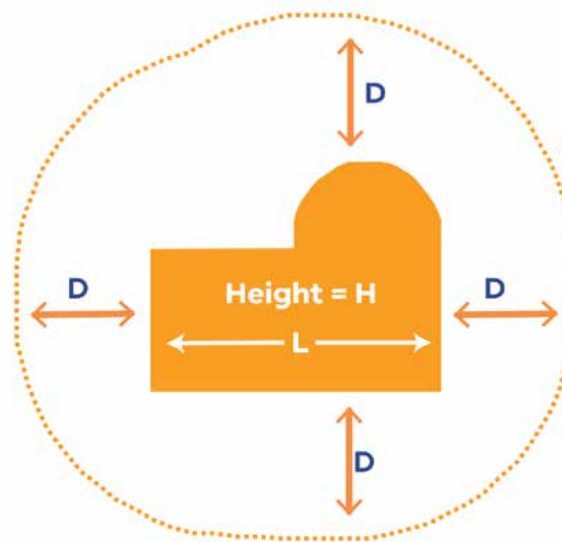


# The pedestrian realm

## Managing wind effects

*Provide a comfortable and safe pedestrian environment by ameliorating the impacts of wind at ground level as part of the design of a building.*

1. Apply the mandatory wind management requirements in *Table 10. Wind effect requirements* to all buildings greater than 16 metres in height.
2. Apply *Figure 25. Wind effect requirement equation* when calculating the requirements in Table 10.
3. A Wind Analysis Report prepared by a suitably qualified person demonstrating that a proposed development will not create unsafe and uncomfortable wind conditions in accordance with the requirements of Table 10 must be presented as part of any application.



**Assessment distance D equals greater of:**  
**L/2 (Half longest width of a building)**  
**OR**  
**H/2 (Half overall height of a building)**

*Figure 25. Wind effect requirement equation*

### Rationale

Accelerated winds near tall buildings can create downdraughts that create discomfort at street level. Downdraughts occur when wind encounters a building and, with nowhere else to go, is pushed up, down and around the sides. The air forced downwards increases the wind speed at street level, which is at best uncomfortable for pedestrians. At worst, it is dangerous: the force of the wind can move people and objects.

Central Geelong's elevated, coastal location makes it highly susceptible to strong winds from many directions, particularly from the north across Corio Bay. As Central Geelong continues to grow, buildings of greater height and scale will increase the potential for wind impacts.

Table 10. Wind effect requirements

Approach	Requirement
Method	<p>A permit cannot be granted for buildings and works which is not in accordance with the assessment method outlined in Figure 25 and the requirements which relate to 'Unsafe wind conditions'.</p> <p>Buildings and works with an overall height equal to, or greater than 16 metres:</p> <ul style="list-style-type: none"> <li>• Must not cause <u>unsafe</u> wind conditions on public land, publicly accessible areas on private land, private open space and communal open space; and</li> <li>• Should achieve <u>comfortable</u> wind conditions as specified in public land and publicly accessible areas on private land.</li> </ul>
Exclusions	Trees and landscaping should not be used to mitigate wind impacts. This does not apply to sitting areas, where trees and landscaping may be used to supplement fixed wind mitigation elements.
Mitigation	Wind mitigation elements should be located within the site boundary, unless consistent with the existing urban context or preferred future development of the area.
Safety requirements	<p><b>Comfortable:</b> Hourly mean wind speed or gust equivalent mean speed (3 second gust wind speed divided by 1.85), from all wind directions combined with probability of exceedance less than 20% of the time, equal to or less than:</p> <ul style="list-style-type: none"> <li>• 3 metres per second for sitting areas,</li> <li>• 4 metres per second for standing areas,</li> <li>• 5 metres per second for walking areas.</li> </ul> <p><b>Unsafe:</b> Annual maximum 3 second gust wind speed exceeding 20 metres per second with a probability of exceedance of 0.1% considering at least 16 wind directions.</p>







# Implementation

**This section outlines how this Framework Plan will be given statutory effect in the Greater Geelong Planning Scheme and how it will be monitored over time.**



## Ensuring design excellence

This Framework Plan has been guided by good urban design and planning principles to help create great places and enable the Central Geelong vision to be realised.

These eight principles are based on the Structure Plan's objectives and strategies and on sound, established urban design practice that achieves economic, social and environmental outcomes.

- Promote architectural and urban design excellence, sustainability, innovation, longevity and creative expression with visionary design, high-quality materials and leading-edge construction methods.
- Promote harmonious fit and compatibility with the existing and planned context, emphasising relationships across variously scaled buildings, parks and open space.
- Conserve and integrate adjacent and onsite heritage buildings and places so new buildings are sympathetic to and compatible with the heritage fabric.
- Consider relationships to other buildings, including the cumulative effect of multiple tall buildings on sunlight, comfort and quality in the public realm.
- Create a safe, comfortable, accessible, vibrant and attractive public realm and pedestrian environment.
- Minimise shadowing and wind impacts and protect access to sunlight and sky view for streets, parks, public and private open space and neighbouring properties.

- Respond appropriately to prominent sites, important views from the public realm and the shape of the skyline to reinforce the city's structure and image.
- Ensure high-quality living and working conditions, including access to public and private open space, interior daylighting, natural ventilation and privacy for building occupants.
- Achieving design excellence outcomes

To support design excellence in Central Geelong, this Framework Plan recommends proponents:

- adopt quality-based selection processes to appoint highly skilled, multidisciplinary teams to develop integrated proposals in line with the vision, objectives, design guidelines and controls established in and by this Framework Plan
- engage early and proactively in pre-application discussions, which may involve a structured, expert design review process at the discretion of the responsible authority
- seek high-quality, expert heritage advice early if a site has or is adjacent to a heritage building or place.

Depending on the complexity and significance of a proposal, the Minister for Planning and/or the City of Greater Geelong (as the responsible authority may request an independent peer review by the Victorian Design Review Panel run by the Office of the Victorian Government Architect.



# The statutory framework

## Planning policy framework

The Planning Policy Framework of the Greater Geelong Planning Scheme contains several clauses and principles that are directly relevant to the planning and development of Central Geelong, including:

- clause 11 Settlement, particularly:
  - clause 11.02-2S, which requires the preparation of structure plans to facilitate the orderly development of urban areas
  - clause 11.03-1S, which requires concentration of major retail, residential and commercial types of development into activity centres that are highly accessible.
- clause 15 Built Environment and Heritage, which requires high-quality urban design and architecture that contributes positively to the local urban character of a place. The clause also gives effect to key design guidelines that will influence built form outcomes, including:
  - the Department of Environment, Land, Water and Planning's 2017 *Urban Design Guidelines for Victoria* and 2017 *Apartment Design Guidelines for Victoria*
  - the Department of Planning and Community Development's 2009 *Urban Design Charter for Victoria*.

Other clauses directly relevant to Central Geelong are clause 16 Housing, clause 17 Economic Development, clause 18 Transport and clause 19 Infrastructure.

## Activity Centre Zone Schedule 1

ACZ1 remains the preferred tool to facilitate the desired spatial and built form outcomes within Central Geelong. The Activity Centre Zone has five purposes:

- to implement the State Planning Policy Framework and the Local Planning Policy Framework, including the Municipal Strategic Statement and local planning policies
- to encourage a mix of uses and the intensive development of the activity centre:
  - as a focus for business, shopping, working, housing, leisure, transport and community facilities
  - to support sustainable urban outcomes that maximise the use of infrastructure and public transport
- to deliver a diversity of housing at higher densities to make optimum use of the facilities and services
- to create through good urban design an attractive, pleasant, walkable, safe and stimulating environment
- to facilitate the use and development of land in accordance with this Framework Plan for the activity centre.

ACZ1 will be applied to the spatial areas as set out in *Figure 7. Central Geelong Activity Centre Zone's new extent*. The expanded area recognises the existing and future retail, office, residential and mixed uses to occur in the area. The schedule will provide a tailored

table of uses that will direct appropriate land uses to the preferred precincts while ensuring permits are required for land uses that may have an off-site or centre-wide impact.

The built form elements in this Framework Plan's Structure Plan and Urban Design Guidelines have been integrated into the ACZ1, which will contain centre-wide design and development provisions supported by precinct-specific guidelines.

## Design and Development Overlay

The Design and Development Overlay (DDO) 17 will be deleted and a new DDO46 focused on Public Use Zone 2 'Education' (PUZ2) will reflect the built form controls in this Framework Plan as they pertain to these sites. Applying DDO46 in concert with the PUZ2 will allow each site to enjoy the 'as of right' use provisions that the public use zones currently provide for, while ensuring that built form outcomes contribute to the vision and objectives in this Framework Plan.

## Development Plan Overlay

The preferred approach to giving statutory planning effect to a master plan is to apply a Development Plan Overlay (DPO) for identified Strategic Development Sites. The DPO is a flexible planning scheme tool that can be used to implement a master plan requirement for each Strategic Development Site.



## Planning scheme amendment

This Framework Plan's vision, objectives and strategies have been thoughtfully integrated into the planning controls explained above, and they have been given statutory effect with an amendment to the Greater Geelong Planning Scheme.

A draft Central Geelong Framework Plan and draft planning scheme amendment package were reviewed by an independent advisory committee established by the Minister for Planning under section 151 of the *Planning and Environment Act 1987* to provide advice and recommendations to inform the final form of both documents.

This Framework Plan and the implemented planning scheme amendment package were informed by the committee's recommendations. The committee's report is available on the Planning Panels Victoria website.

# Delivering the actions

## Revitalising Central Geelong Partnership

This Framework Plan contains statutory and non-statutory actions necessary to implement the vision, objectives and strategies in it. The actions in this Framework Plan come with general timeframes and nominate lead and partner agencies from the Revitalising Central Geelong (RCG) Partnership. The RCG Partnership comprises senior officials from state departments and agencies and the City of Greater Geelong, who work together to implement the actions in this Framework Plan.

The RCG Partnership will be responsible for oversight delivery of the non-statutory planning actions in this Framework. The actions will be delivered progressively in line with each agency's annual budget.

The Minister for Planning will continue to oversee, on behalf of the Victorian Government, the RCG Partnership and its delivery program. The Minister for Planning will be supported by the Geelong Authority, which has and will continue to have a strategic role that supports Central Geelong's long-term growth potential. It will continue to advise the Minister for Planning about strategies to attract investment to Central Geelong and major planning applications to help create jobs and drive growth in Geelong.

## Monitoring and review

Monitoring of the implementation of actions in this Framework Plan will include:

- monitoring of the supply of commercial and residential floor space across Central Geelong to ensure capacity is being provided to meet growth needs
- monitoring the diversity of commercial uses and the employment base as a proportion of overall growth to ensure floor space for job creation occurs
- ongoing assessment and monitoring of community needs and services undertaken as part of the Revisitation Central Geelong Partnership
- ongoing review of the effectiveness of local planning policy and other planning scheme provisions in achieving the vision in the Structure Plan.







# Appendices

**This section presents further information that supports the content of this Framework Plan.**

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## Appendix B: Reference documents

- *Arts and Cultural Precinct Masterplan 2017*
- *Central Geelong Action Plan 2013*
- *Central Geelong Car Parking Strategy (Draft) 2018*
- *Central Geelong Community Infrastructure Strategy 2017*
- *Central Geelong Live Music Action Plan 2017*
- *Central Geelong Public Realm Framework 2017*
- *Central Geelong Utility Infrastructure Strategy 2017*
- *Central Geelong Waterfront Masterplan 2011*
- *Central Geelong West End Action Plan 2017*
- *City of Greater Geelong Environment Management Strategy 2014–2017*
- *City of Greater Geelong Heritage Strategy 2017–2021*
- *City of Greater Geelong Open Space Social Infrastructure Plan 2020*
- *City of Greater Geelong Urban Forest Strategy 2015–2025*
- *City of Greater Geelong's Access and Inclusion Action Plan 2018–22*
- *Council Plan 2018–2022*
- *G21 Regional Growth Plan 2013*
- *Geelong Hospital Precinct Plan (in progress)*
- *Geelong Play Strategy 2011*
- *Greater Geelong and Bellarine Tourism Development Plan 2019*
- *Greater Geelong Planning Scheme - Clause 21.09 Central Geelong policy*
- *Greater Geelong: A Clever and Creative Future 2017*
- *GSSC Digital Learning Hub Business Case 2015*
- *Housing Diversity Strategy 2008*
- *Laneways Action Plan 2018*
- *Making Geelong Accessible 2017*
- *Public Art Strategy 2017*
- *Revitalising Central Geelong Open Space Network Strategy 2018*
- *Revitalising Central Geelong Action Plan 2016*
- *Our community places, spaces and services: A social infrastructure plan for the City of Greater Geelong, Generation One: 2020 - 2023, 2020*
- *The City of Greater Geelong Retail Strategy 2020–36*
- *The City of Greater Geelong Settlement Strategy 2020*
- *The City of Greater Geelong Social Housing Plan 2020–41, 2020*
- *Victoria in Future 2019*



# Appendix C: Floor space analysis

**SGS Economics and Planning (SGS) were engaged to undertake a land use demand and market assessment analysis for Central Geelong. A more detailed overview of the methodology, approach and conclusions can be found in *Central Geelong Land Use and Market Assessment Report – Update April 2022*.**

## Determining floor space capacity needs

Three scenarios have been prepared to help provide an estimate of the amount of residential and commercial floor space needed in Central Geelong by 2050. Each scenario uses the following base components to define potential demand:

- a municipal-wide population growth rate to provide a municipal-wide projected population number extrapolated to 2050
- Central Geelong's potential share of the municipal-wide population in 2050
- Central Geelong's share of municipal-wide share of employment in 2050.

The upside and optimistic scenarios have been developed considering a range of market analytics, proposed new infrastructure and other investments, and further progression of a range of policy and development aspirations for Central Geelong. Benchmarking against comparable Australian regional centres has also occurred.

Employment scenarios have been converted to floor space using ratios of floor space to jobs to identify an employment floor space demand range for Central Geelong from 2020 to 2050. Residential scenarios have converted the population to floor space by using the average household size in Central Geelong to identify the level of residential floor space demand from 2020 to 2050.



## Base case scenario

This scenario presents a conservative approach to projecting future growth and demand using *Victoria in Future 2019* population projections, a focus on doubling the resident population and sustaining a decrease in the municipal share of employment within Central Geelong.

This scenario uses:

- an average annual population growth rate of 2.1%, consistent with *Victoria in Future 2019* forecasts extrapolated out to 2050
- a share of the LGA population in Central Geelong of 1.7% in 2050
- a share of the LGA employment in Central Geelong of 19% in 2050.

Under this scenario, by 2050:

- the share of the population within Central Geelong is projected to increase to 7,000 new residents, which would require an additional 210,621 square metres of new residential floor space
- the share of total jobs would increase to 38,000, which would require an additional 414,000 square metres of new commercial floor space.

## Upside scenario

This scenario presents a moderate approach to projecting future growth and demand using a more optimistic population growth rate, a doubling of Central Geelong's share of population and a small decrease in the municipal share of jobs within Central Geelong.

This scenario uses:

- a population growth rate of 2.5% a year for the Greater Geelong LGA, consistent with Scenario D of *The City of Greater Geelong Settlement Strategy 2020*.
- a share of the LGA population in Central Geelong of between 2.0% and 2.5% in 2050
- a share of the LGA employment in Central Geelong 20% in 2050.

Under this scenario, by 2050:

- the share of the population within Central Geelong is projected to increase to between 11,000 and 13,000 new residents, which would require between 400,000 and 550,000 square metres of new residential floor space.
- the share of total jobs would increase to 55,000, which would require an additional 862,000 square metres of new commercial floor space.

## Optimistic scenario

This scenario provides for the same optimistic, sustained population growth rate as in the Upside scenario but provides for a tripling of the share of population and a slight decrease in the municipal share of jobs within Central Geelong.

This scenario uses:

- a population growth rate of 2.5% a year for the Greater Geelong LGA, consistent with Scenario D of *The City of Greater Geelong Settlement Strategy 2020*
- a share of LGA population in Central Geelong of 2.5% to 3.0% in 2050
- a share of LGA employment in Central Geelong of 22% in 2050.

Under this scenario, by 2050:

- the share of the population within Central Geelong is projected to increase to between 13,000 and 16,000 new residents, which would require between 541,000 and 703,000 square metres of new residential floor space
- the share of total jobs would increase to 59,000, which would require an additional 967,000 square metres of new commercial floor space.

Table 11. Central Geelong population and employment scenarios, 2020–50

Base case		Upside		Optimistic	
Employment	Residents	Employment	Residents	Employment	Residents
38,000	7,000	55,000	11,000 to 13,000	59,000	13,000 to 16,000

Source: Central Geelong land Use and Market Update (2022), SGS Planning and Economics.

Table 12. Central Geelong additional floor space scenarios, 2020–50

Base Case		Upside		Optimistic	
Employment	Residential	Employment	Residential	Employment	Residential
+414,000	+210,000	+862,000	+399,000 to 548,000	+967,000	+541,000 to 703,000
+624,000		+1,261,000 to 1,410,000		+1,508,000 to 1,670,000	

Source: Central Geelong land Use and Market Update (2022), SGS Planning and Economics.

# Applying floor space capacity outcomes

The methodology for undertaking the capacity model is detailed in the *Draft Central Geelong Framework Plan Background Report*. Two, three-dimensional GIS models were used to assist in determining the appropriateness of built form planning controls to meet future capacity needs.

## The planning control model

First, a planning control model was developed to test and visualise the application of the following built form controls:

- preferred maximum building heights
- preferred maximum street wall heights
- preferred minimum tower separation distances
- preferred street corner outcomes
- preferred front, side and rear setbacks
- mandatory overshadowing controls.

## The floor space model

Second, a floor space model was developed, using the planning control model as its base, to test the ability of the above planning controls for each of the floor space scenarios in *Table 3. Central Geelong population, employment and required floorspace to 2050*.

The floor space model includes a range of sites with future development potential. The sites were selected and categorised using detailed criteria and identified as strategic development sites, primary sites or secondary sites.

## Built form testing

Testing to determine the adequacy of the proposed planning controls against each of the floor space scenarios considered, among other things:

- the current gross floor area of existing buildings
- future floor space to be provided by sites under construction
- future floor space based on approved but yet-to-be-constructed development
- the probability discounting of a given site being developed in the next 30 years
- the efficiency discounting of a given site where the floor space yield may be reduced based on the design response to site conditions or limitations.

An iterative review and refinement process was undertaken using both models to adjust the planning controls to ensure there would be sufficient capacity while maintaining the long-term urban design outcomes intended by this Framework Plan.

## Caveats and conclusions

The projections and corresponding analysis were developed to guide the establishment of planning controls to ensure the right balance is achieved between the growth aspirations and the future urban form of Central Geelong.

This Framework Plan seeks to apply, in the main, discretionary built form controls which provide the potential to deliver development outcomes more than the modelled capacity across Central Geelong.

All the scenarios are long-range forecasts. Real-time monitoring of floor space demand and supply, influenced by the COVID-19 and other macro-economic trends and policy impacts, will be important to inform regular reviews of this Framework Plan.

# Applying plot ratios outcomes

## The methodology

A plot ratio is a ratio between achievable floor space and site area. In determining the plot ratio for each strategic development site, the following methodology was followed using the parameters below.

The Plot Ratio for each site was calculated by determining the achievable floor space for each site. In summary this is based on a podium and tower typology established by the proposed built form controls.

The podium floor space was calculated by multiplying the podium area (usually site area minus any required pedestrian links) by the number of stories that could be achieved within the proposed preferred maximum street wall heights.

The tower floor space was calculated by multiplying the tower area (1,000 square metres) by the number of stories that could be achieved within the proposed preferred maximum building heights (above the podium).

A more detailed explanation of each of the methodology's component parts is explained below and shown illustratively in *Figure 26. Plot ratio component parts*

### Podium and tower building form typology

This typology has been used as it represents a built form outcome reflecting a large podium with tower elements above. The relevant built form controls include, building height, maximum floor plate size, street wall heights, setbacks etc found in this Framework Plan, and result in the potential building envelope for each site.

### Application of proposed preferred maximum building heights

The preferred maximum building height outlined in *Figure 8. Central Geelong preferred maximum building height'* combined with the preferred maximum floor plate (d) was used along with a floor-to-floor height of 3.5 metres (e) to determine the gross floor area of the tower element.

### Application of proposed preferred maximum street wall heights

The preferred maximum street wall height nominated for each site is outlined in *Figure 19. Preferred maximum street wall heights*. The nominated street wall height has been used to determine the number of stories within a potential podium to determine the gross floor area.

### Application of a maximum tower floor plate and placement

A maximum tower floor plate of 20 metres by 50 metres (1,000 square metres) has been applied to

determine the potential total floorspace of each tower element above a podium. The number and placement of each tower element takes into consideration the built form controls that require a 12 metre / 20 metre separation between tower elements, and application of a relevant setback above a street wall (between 4.5 metres and 10 metres depending on overall building height).

The above preferred maximum tower floorplate was informed by common dimensions and floor plate areas of recent permits in Geelong as well as Fishermans Bend, as well as an understanding of relevant design guidelines for apartments, building codes, buildings recognised for design quality and general commercial viability. The above dimensions have been applied as it is applicable for both commercial and residential towers in Geelong, in recognition that the different commercial market in Geelong compared to Melbourne results in a smaller commercial floor plate.

### Application of storey heights

Application of a 4-metre floor-to-floor ground level storey height has been applied to all sites. The dimensions chosen reflects the Framework Plan's objectives for active uses on the ground floor and adaptability above. On all floors above the ground floor, a 3.5 metre floor-to-floor (commercial) storey height has therefore been applied to both the podium and tower elements.

### **Application of proposed pedestrian links**

A number of identified SDS's contain proposed pedestrian links. These pedestrian links support the movement of people in both a north south and east west throughout Central Geelong contributing to and help make the pedestrian experience easier and more comfortable. For strategic development sites, a pedestrian link 10 metres wide was factored in, to determine the overall gross floor area of a site. The location of each pedestrian link is approximate and aligned with *Figure 26. Plot ratio component parts*.

### **Application mandatory overshadowing provisions**

The open space and overshadowing controls in this Framework Plan were used with other controls to determine the building envelope. The position and height of potential towers were adjusted to fit within this envelope to avoid any conflict and ensure the gross floor area per site reflects what is achievable within the mandatory overshadowing controls

## **General notes and assumptions**

### **Existing buildings**

The concluded plot ratios anticipate that the complete development of a site will occur and would comply with the planning controls. The retention of existing buildings would affect the ability to achieve the proposed plot ratios, particularly if they were well within the building envelope achievable under the built form controls or had taller floor-to-floor levels, as explained above.

### **Proposed building controls**

Each plot ratio is to be applied in conjunction with other built form requirements. Each plot ratio reflects a building envelope that is envisaged by this Framework Plan. Therefore, increasing plot ratios beyond those proposed would be likely to result in outcomes inconsistent with the methodology and the objectives of this Framework Plan.

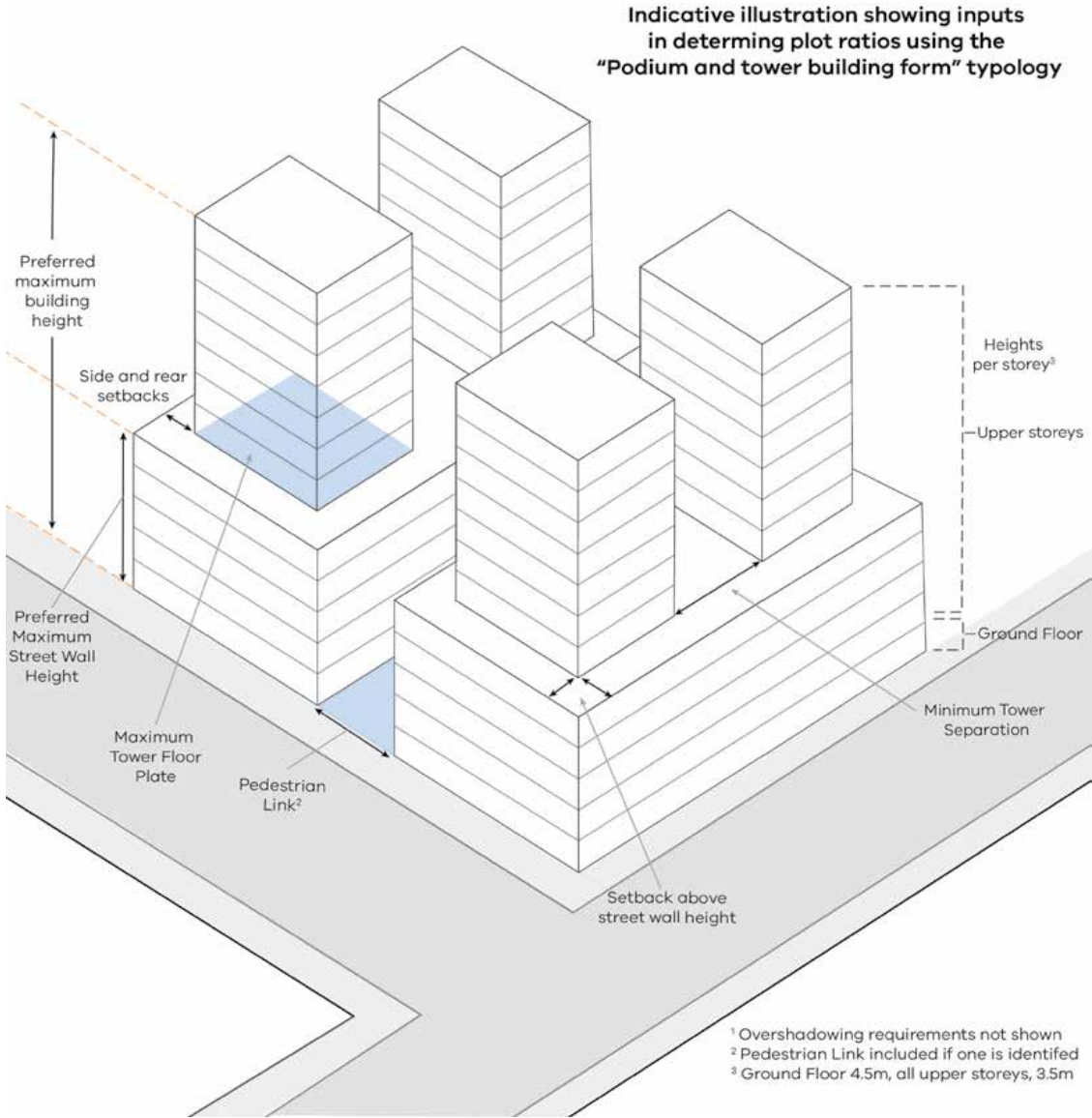
### **Open space**

Future open space allowances have not been considered in the calculation of proposed plot ratios, except for Haymarket, where a 40% allocation of land was considered. This Framework Plan identifies where new open space is needed. For the remaining strategic development sites, this Framework Plan encourages the voluntary inclusion of publicly accessible open space, plazas or forecourts. The consideration of new open space would affect the overall plot ratio noted for all sites.

### **Station Precinct**

The development of a plot ratio for Geelong Station will occur as part of the proposed master planning process envisaged by this Framework Plan. The site has government tenants and various forms of Crown land that will need to be more accurately determined, and built form assessments will be needed to determine an appropriate floor area ratio for the whole area or parts of it. Heritage-listed buildings and the proposed future forecourt will need to be considered in this assessment.

Figure 26. Plot ratio component parts



# Appendix D: Open space locations

